



# City of Coral Gables Board of Architects Staff Report

**Property:** Gables Village

**Architect:** de la Guardia Victoria Architects & Urbanists

**Application:** Townhouses, Flats, Lofts, & Duplexes

**BOA Permit:** AB-21-08-7672

**Requests:**

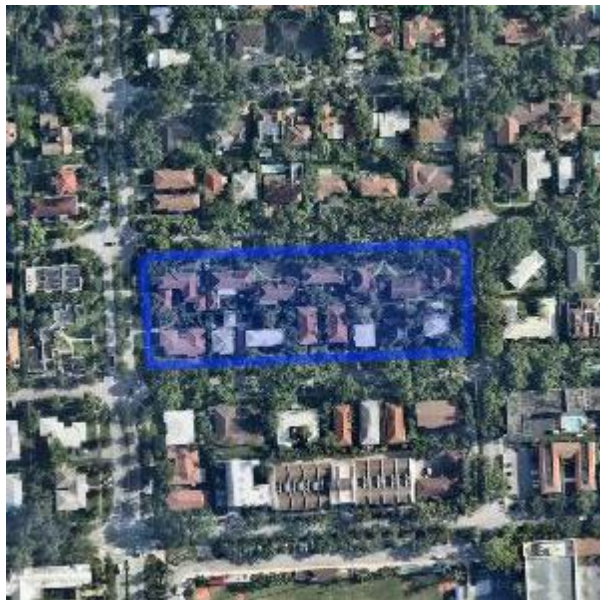
1. Planned Area Development (PAD)
2. Mediterranean Bonus Levels 1 & 2

**Location:** 504 Malaga Ave & 525 Santander Ave

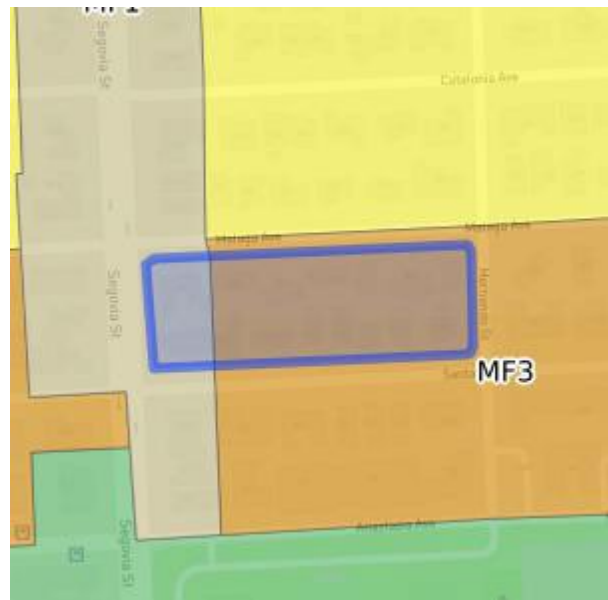
## APPLICATION REQUESTS

The Applicant is requesting review of a proposed multi-family redevelopment, located on the block bounded by Malaga, Hernando, Santander, and Segovia. The project site is comprised of platted lots 1 through 22 in Block 28, Coral Gables Biltmore Section. It is approximately 113,310 square feet (2.6 acres). The project proposes a total of 48 residential units, including 24 flats, 4 lofts, 16 townhouses, and 4 duplex units. The proposed buildings heights vary between 30 feet at 2-stories and 45 feet at 4-stories.

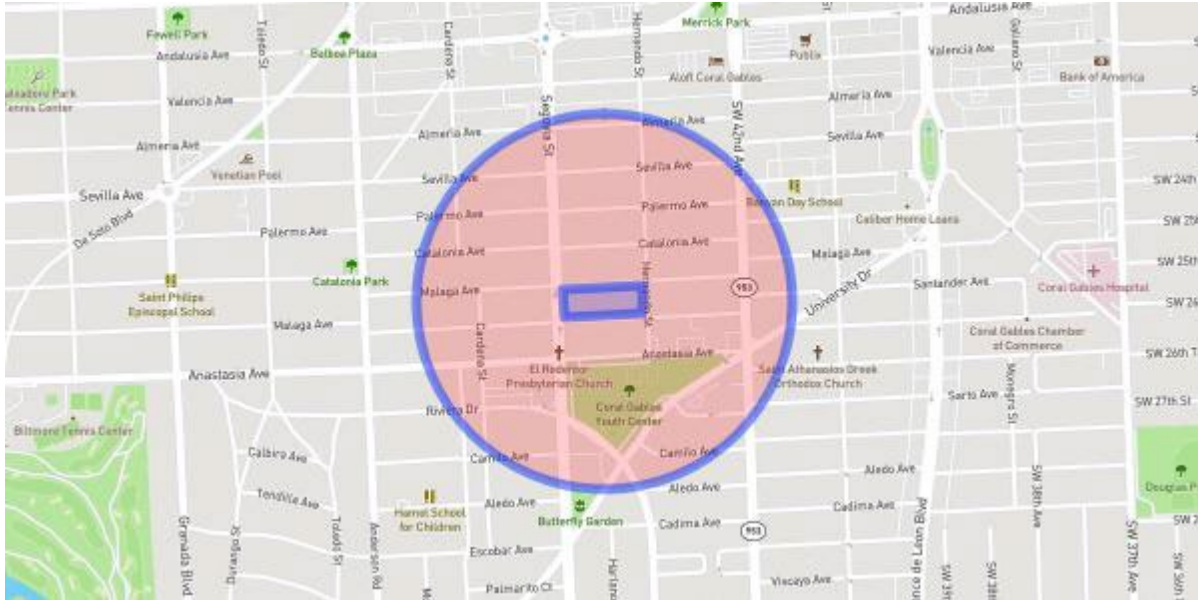
The building site has two Land Use designations of Residential Multi-Family Low Density and Duplex, and is currently zoned Multi-Family 3 (MF3) and Multi-Family 1 Duplex (MF1).



Aerial



Zoning: MF1 and MF3



¼ mile (5-minute walk) radius

## 1. Planned Area Development (PAD)

The minimum site area required for PAD shall not be less than one (1) acre; the project site is more than one acre (2.6 acres). The applicant is proposing a PAD designation to allow flexibility in the following requirements of the underlying zoning district of Multi-family 3 (MF3) per Section 2-500(B)(3)(j):

- **Parking Setback:** Adjust the front parking setback from the required 20 feet to 0 feet on the ground floor parking level facing Santander.
- **Facing of buildings:** Adjust the facing of the proposed lofts to face Hernando.
- **Height abutting MF1:** Allow building height abutting MF1 of 44 feet and 4 stories per Section 2-500(B)(3)(i), when 35 feet and 3 stories is allowed.

The request for the Planned Area Development (PAD) provides flexibility from the regulations of the underlying zoning district. Section 2-500 of the Zoning Code outlines the purpose of the PAD as follows:

- a. Allow opportunities for more creative and imaginative development than generally possible under the strict applications of these regulations so that new development may provide substantial additional public benefit.
- b. Encourage enhancement and preservation of lands which are unique or of outstanding scenic, environmental, cultural and historical significance.
- c. Provide an alternative for more efficient use and, safer networks of streets, promoting greater opportunities for public and private open space, and recreation areas and enforce and maintain neighborhood and community identity.
- d. Encourage compatible and coordinated development of the site, through the use of a variety of architectural solutions to promote Mediterranean architectural attributes, promoting variations in bulk and massing, preservation of natural features, scenic areas, community facilities, reduce land utilization for roads and separate pedestrian and vehicular circulation systems and promote urban design amenities.

- e. Require the application of professional planning and design techniques to achieve overall coordinated development eliminating the negative impacts of unplanned and piecemeal developments likely to result from rigid adherence to the standards found elsewhere in these regulations.

Per Section 14-206.3(C), *General Procedures for Planned Area Development*, the applicant shall submit plans to the Board of Architects for review and preliminary approval prior to filing a formal application for Planning & Zoning Board review.

In addition to the consideration of granting flexibility to the underlying zoning requirements, the Board of Architects should also review that the PAD meets the required design requirements, per Section 2-500(B)(3)(h):

- i. Architectural relief and elements (i.e. windows, cornice lines, etc.) shall be provided on all sides of buildings, similar to the architectural features provided on the front façade.
- ii. Facades in excess of one hundred and fifty (150) feet in length shall incorporate design features such as: staggering of the façade, use of architectural elements such as kiosks, overhangs, arcades, etc.
- iii. Parking garages shall include architectural treatments compatible with buildings and structures which occupy the same street;
- iv. Where necessary and appropriate to enhance public pedestrian access, no block face shall have a length greater than two hundred and fifty (250) feet without a public pedestrian passageway or alley providing through access; and
- v. All buildings, except accessory buildings, shall have their main pedestrian entrance oriented towards the front or side property line.

## SITE PLAN INFORMATION

Standard	Required/Permitted		Proposed
Building Site			
	1 acre for PAD		2.6 acres
Lot Coverage			
MF1 (35% / 45%)	9,823 square feet max.		56,977 sq.ft. (50.3%)
MF3 (70%)	64,037 square feet max.		
Building Site Width	200 feet for PAD		458 feet on Santander
Open Space			
PAD (20% w/in site)	22,662 (w/in site)		42,662 w/in site only (37.7%)
MF1 (40%)	8,731 (w/in site)		
MF3 (25% incl. right of way)	28,327.5 (may include row)		
Density	MF1	MF3	
	4.5 units (9 per acre)	42 units (20 per acre)	48 units
with Med Bonus I & II	n/a	52.5 units (25 per acre)	
Setback			
Principal front	25ft	10ft	25ft (MF1) & 10ft (MF3)
Side street	15ft	10ft	15ft (MF1) & 10ft (MF3)

Parking setback per MF3	30 feet from the property line		10 feet from property line
<b>Building Height</b>	<b>MF1</b>	<b>MF3</b>	
Principal Building	2 stories/ 30ft	45ft	30ft (MF1) 45ft (MF3)
<b>Parking</b>			
Duplex @1/unit (4 units)	4		8
Townhouse @2/unit (16 units)	32		32
2BR @1.75/unit (30 units)	49		52
<b>TOTAL</b>	<b>85</b>		<b>92</b>

## 2. Coral Gables Mediterranean Bonus Levels 1 and 2

The project proposes 48 residential units, with 92 parking spaces. The proposed building height varies from 30 to 45 feet.

According to Section 5-201, *Coral Gables Mediterranean Style Design Standards*, MF3 residential applications are required to satisfy all the requirements in Table 1, “Required Standards” and 6 of the 12 qualifications in Table 2, “Architectural and Public Realm Standards” to secure the maximum bonuses of additional density. In addition, all applications shall be reviewed for compliance of the provisions of Section 5-202, *Coral Gables Mediterranean Architecture Design*.

Coral Gables Mediterranean Level 1 and Level 2 floor area ratio (FAR) and additional stories do not apply to MF3 zoned properties. The density bonus will allow the additional 2 units to be 21 units per acre, when a maximum of 25 units per acre is allowed with secured bonuses.

## FINDINGS OF FACT

### 1. Planned Area Development (PAD)

Section 14-206 of the Zoning Code states the required findings for a proposed PAD project is as follows:

A. *In what respects the proposed plan is or is not consistent with the stated purpose and intent of the PAD regulations.*

**Staff comments:** The proposed project is consistent with the stated purpose and intent of the PAD regulations to provide high quality development and mix of uses and densities through flexible guidelines. The flexibility allowed under the PAD to match the building heights allows a cohesive village of different housing types that is compatible with the surrounding low-rise apartment, duplex, and single-family buildings. The reduction of the parking setback will activate Santander similarly to the required street-facing residential units and fulfil the purpose of the required parking setback with the proposed Art in Public Places of various benches, fountains, free street library, and other pedestrian amenities.

B. *The extent to which the proposed plan departs from the zoning and subdivision regulations otherwise applicable to the subject property, including but not limited to density, size, area, bulk and use, and the reasons why such departures are or are not deemed to be in the public interest.*

**Staff comments:** The departures from the zoning of the proposed project include the reduction of the parking setback, adjustment of MF3 building height abutting MF1, and adjustment of MF3 facing on Hernando. The departures allow for a cohesive village development and unique public art pedestrian activation on Santander.

*C. The extent to which the proposed plan meets the requirements and standards of the PAD regulations.*

**Staff comments:** The proposed plan meets the requirements and standards of the PAD regulations such as contiguous unified parcel with a minimum lot width of two hundred (200) ft. and minimum lot depth of one hundred (100) ft. and a minimum site area of no less than an acre.

*D. The physical design of the proposed PAD and the manner in which said design does or does not make adequate provision for public services, provide adequate control over vehicular traffic, provide for and protect designated common open areas, and further the amenities of light and air, recreation and visual enjoyment.*

**Staff comments:** The physical design of the proposed PAD provides for an additional 4 open spaces in the forms of courtyards and garden courts that face Malaga and Santander, allowing for pedestrian circulation and visual enjoyment.

*E. The compatibility of the proposed PAD with the adjacent properties and neighborhood as well as the current neighborhood context including current uses.*

**Staff comments:** The proposed massing and scale of the PAD is compatible with the adjacent multi-family properties in the area with relatively small building footprints and ample open space massing.

*F. The desirability of the proposed PAD to physical development of the entire community.*

**Staff comments:** The redevelopment of the existing properties introduces new housing stock in high quality design within the established neighborhood.

*G. The conformity of the proposed PAD with the goals and objectives and Future Land Use Maps of the City of Coral Gables Comprehensive Plan.*

**Staff comments:** The proposed PAD is “consistent” with the CP’s Goals, Objectives and Policies that address the City’s objectives for encouraging high quality, creative design and site planning that is compatible with the City’s architectural heritage, surrounding development, public spaces, and open spaces.

## **2. Mediterranean Bonus**

Applications shall be required to satisfy all of the requirements in Table 1, “Required Standards” in order to secure bonuses Zoning Code **Section 5-201.B.1.:**

STANDARD	STAFF EVALUATION
<b>1. Architectural elements on building facades:</b> Similar exterior architectural relief elements shall be provided on all sides of all buildings. No blank walls shall be permitted unless required pursuant to	Complies. The proposed buildings have architectural elements and openings on all facades.

<p>applicable City, State and Federal requirements (i.e., Fire and Life Safety Code, or other applicable code.)</p> <p>Parking garages shall include exterior architectural treatments compatible with buildings or structures that occupy the same property and/or street.</p>	
<p><b>2. Architectural relief elements at street level:</b></p> <p>On any building facades fronting streets, where an adjoining pedestrian sidewalk is located, one (1) or more of the following design features shall be included at the street level:</p> <ul style="list-style-type: none"> <li>a. Display windows or retail display area;</li> <li>b. Landscaping; and/or</li> <li>c. Architectural relief elements or ornamentation.</li> </ul>	<p>Complies.</p> <p>The project's ground floor offers ground floor residential units, high quality benches, and front yard landscaping.</p>
<p><b>3. Architectural elements located on the top of buildings:</b></p> <p>Exclusion from height. The following shall be excluded from computation of building height in MX Districts:</p> <ul style="list-style-type: none"> <li>a. Air-conditioning equipment room.</li> <li>b. Elevator shafts.</li> <li>c. Elevator mechanical equipment rooms.</li> <li>d. Parapets.</li> </ul> <p>Roof structures used only for ornamental and/or aesthetic purposes not exceeding a combined area of twenty-five (25%) percent of the floor area immediately below. Such exclusion shall be subject to the provisions that no such structure shall exceed a height of more than twenty-five (25) feet above the roof, except for commercial buildings in the Central Business District (CBD) where no such structure shall exceed one-third (1/3) of the allowable total building height.</p>	<p>Not applicable.</p> <p>Proposed project is not in a MX District.</p>
<p><b>4. Bicycle storage:</b></p> <p>To encourage the use of bicycles, bicycle storage facilities (racks) shall be provided. A minimum of five (5) bicycle storage spaces shall be provided for each two hundred and fifty (250) parking spaces or fraction thereof.</p>	<p>Complies.</p> <p>Long-term bicycle parking available on the ground floors of the duplexes, lofts, and flats.</p>
<p><b>5. Building facades:</b></p> <p>Facades in excess of one hundred and fifty (150) feet in length shall incorporate vertical breaks, stepbacks or variations in bulk/massing at a minimum of one hundred (100) foot intervals.</p>	<p>Complies.</p> <p>The maximum facades lengths are 180 feet. Multiple vertical breaks and variations in bulk/massing are provided at a maximum of 50-foot intervals to ensure a pedestrian scale.</p>
<p><b>6. Building lot coverage:</b></p> <p>No minimum or maximum building lot or ground coverage is required</p>	<p>Complies.</p> <p>Building lot coverage provided for the entire PAD is 50.3%, when the maximum of the underlying zoning</p>

	district is 70%.
<b>7. Drive through facilities.:</b> Drive through facilities including banking facilities, restaurants, pharmacies, dry cleaners, or other drive-through businesses are prohibited access to/from Ponce de Leon Boulevard from S.W. 8th Street to Bird Road, Miracle Mile from Douglas Avenue to LeJeune Road, and Alhambra Circle from Douglas Avenue to LeJeune Road.	Not applicable.
<b>8. Landscape open space area:</b> Each property shall provide the following minimum ground-level landscape open area (percentage based upon total lot area): a. Five (5%) percent for nonresidential properties; b. Ten (10%) percent for mixed use properties; and c. Twenty-five (25%) percent for residential properties. The total area shall be based upon the total lot area. This landscape area can be provided at street level, within the public right-of-way, planter boxes, planters, and other ground-floor locations.	Complies. Open space provided within the property is 37.7%.
<b>9. Lighting, street:</b> Street lighting shall be provided and located on all streets/rights-of-way. The type of fixture shall be the approved City of Coral Gables light fixture. The location, spacing, and other specifications shall be subject to review and approval by the Department of Public Works.	Complies. Consider replacing existing cobra lighting with pedestrian-scaled lighting on Santander, Malaga, and Hernando.
<b>10. Parking garages:</b> Ground floor parking as a part of a multi-use building shall not front on a primary street. ADA parking is permitted on the ground floor. Ground floor parking is permitted on secondary/side streets and shall be fully enclosed within the structure and/or shall be surrounded by retail uses and/or residential units. Ground floor parking is permitted on alley frontages. Parking facilities shall strive to accommodate pedestrian access to all adjacent street(s) and alleys.	Not applicable. Proposed buildings are not multi-use.
<b>11. Porte-cocheres:</b> Porte-cocheres are prohibited access to/from Ponce de Leon Boulevard from S.W. 8th Street to Bird Road, Miracle Mile from Douglas Avenue to LeJeune Road, and Alhambra Circle from Douglas Avenue to LeJeune Road.	Not applicable.
<b>12. Sidewalks/pedestrian access:</b> All buildings, except accessory buildings, shall have their main pedestrian entrances oriented towards adjoining streets. Pedestrian pathways and/or sidewalks shall be provided	Complies.  Proposed buildings adjoining streets provide pedestrian entrances, courtyard entrances, and sidewalk

from all pedestrian access points and shall connect to one another to form a continuous pedestrian network from buildings, parking facilities, parking garages entrances, and other structures. Wherever possible pathways shall be separated from vehicular traffic.	access.
<b>13. Soil, structural:</b> Structural soil shall be utilized within all rights-of-way for all street level planting areas with root barriers approved by the Public Works Landscape Division.	Complies. Coordinate with Public Works staff.
<b>14. Windows on Mediterranean buildings:</b> Mediterranean buildings shall provide a minimum window casing depth of four (4) inches as measured from the face of the building.	Complies. Four inches provided from face of building to window glass.

All applications desiring bonuses shall satisfy a minimum of six (6) of the twelve (12) qualifications of Table 2, Zoning Code Section **5-201.D. Architectural and Public Realm Standards** to secure a bonus under these provisions:

Standard	Staff Evaluation
<b>1. Arcades and/or loggias:</b> Arcades, loggias or covered areas constructed adjacent, parallel, and/or perpendicular to building to provide cover and protection from the elements for pedestrian passageways, sidewalks, and other walkways thereby promoting pedestrian passage/use. Limitations of encroachments on corners of buildings may be required to control view corridors and ground stories building bulk and massing. Awnings or other similar items do not satisfy these provisions.	Not provided. Arcades and loggias are not appropriate for a multi-family building.
<b>2. Building rooflines:</b> Incorporation of horizontal and vertical changes in the building roofline.	Complies. Roof changes are articulated horizontally and vertically.
<b>3. Building stepbacks:</b> Stepbacks on building facades of the building base, middle and/or top facade to further reduce the potential impacts of the building bulk and mass.	Complies. Stepbacks are provided for larger buildings that reduces the bulk and mass.
<b>4. Building towers:</b> The use of towers or similar masses to reduce the mass and bulk of buildings.	Complies. Multiple towers are provided in the larger buildings.
<b>5. Driveways:</b> Consolidation of vehicular entrances for drive-through facilities, garage entrances, service bays and loading/unloading facilities into one (1) curb cut per	Not provided. The proposed PAD encompasses the entire block and provides narrow 1-way driveways which is more



street to reduce the amount of vehicular penetration into pedestrian sidewalks and adjoining rights-of-way.	compatible and less intrusive with the neighborhood context.
<b>6. Lighting of landscaping:</b> Uplighting of landscaping within and/or adjacent to pedestrian areas (i.e., sidewalks, plazas, open spaces, and other public spaces).	Complies. Plans indicate that lighting will comply with landscape requirements.
<b>7. Materials on exterior building facades:</b> The use of natural materials shall be incorporated into the base of the building on exterior surfaces of building. This includes the following: marble, granite, keystone, and other types of natural stone.	Complies. The buildings incorporate Coralina entry steps, cast stone arches, cornices, column bases, motifs and other ornamental architectural features.
<b>8. Overhead doors:</b> If overhead doors are utilized, the doors are not directed towards residentially zoned properties.	Not applicable.
<b>9. Paver treatments:</b> Inclusion of paver treatments in all of the following locations: a. Driveway entrances minimum of ten (10%) percent of total paving surface. b. Sidewalks. Minimum of twenty-five (25%) percent of total ground level paving surface. The type of paver shall be subject to Public Works Department review and approval. Poured concrete color shall be Coral Gables Beige.	Complies. Plans include paving precedents and indicate that paving will comply with paving requirements.
<b>10. Pedestrian amenities:</b> Pedestrian amenities on both private property and/or public open spaces including a minimum of four (4) of the following: a. Benches. b. Expanded sidewalk widths beyond the property line. c. Freestanding information kiosk (no advertising shall be permitted). d. Planter boxes. e. Refuse containers. f. Public art. g. Water features, fountains and other similar water features. Ground and/or wall mounted. Above amenities shall be consistent in design and form with the City of Coral Gables Master Streetscape Plan.	Complies. Four public art benches, expanded sidewalks beyond the property line, public art, and fountains are all provided on the ground floor.
<b>11. Pedestrian passthroughs/paseos on properties contiguous to alleys and/or streets:</b> Pedestrian pass-throughs provided for each two	Complies. Two landscaped pedestrian passthroughs are provided through a

<p>hundred and fifty (250) linear feet or fraction thereof of building frontage provided on properties contiguous to alleys and/or streets or other publicly owned properties. Buildings less than two hundred and fifty (250) feet in size shall provide a minimum of one (1) pass through. The pass-throughs shall be subject to the following:</p> <ul style="list-style-type: none"> <li>a. Minimum of ten (10) feet in width.</li> <li>b. Include pedestrian amenities as defined herein. In lieu of providing one (1) pass-through of ten (10) feet in width every two hundred and fifty (250) feet of building frontage, two (2) pass-throughs can be combined to provide one (1) twenty (20) foot wide pass-through.</li> </ul>	<p>series of courtyards and garden courts to connect Malaga and Santander. An additional 20-foot-wide pedestrian via is also provided within the block.</p>
<p><b>12. Underground parking:</b> The use of underground (below grade level) parking, equal in floor area of a minimum of seventy-five (75%) percent of the total surface lot area. Underground parking shall be located entirely below the established grade as measured from the top of the supporting structure and includes all areas utilized for the storage of vehicles and associated a circulation features.</p>	<p>Not applicable. No underground parking proposed.</p>

All applications for development approval shall be required to satisfy **Section 5-202** of the Zoning Code, *Coral Gables Mediterranean Architecture Design*, to include design elements and architectural styles of the following buildings:

- a. H. George Fink Offices, 2506 Ponce de Leon Boulevard.
- b. The Colonnade Building, 169 Miracle Mile.
- c. Douglas Entrance, 800 Douglas Road.
- d. Coral Gables Elementary School, 105 Minorca Avenue.
- e. Granada Shops/Charade Restaurant, 2900 Ponce de Leon Boulevard (demolished).
- f. San Sebastian Apartments, 333 University Drive.
- g. Coral Gables City Hall, 405 Biltmore Way.
- h. Biltmore Hotel, 1200 Anastasia Avenue.

## SUMMARY OF REQUESTS

Per Zoning Code, the Board of Architects shall act as a recommending and a decision-making Board for the following:

### 1. Planned Area Development (PAD):

- Allows flexibility with literal requirements of the Zoning Code for equivalent or increased public benefit
- PAD designation to provide flexibility to reduce the required 30 feet parking setback,

- BOA approval needed prior to filing a formal application for Planning & Zoning Board review, per Section 14-206.3(C) *Board of Architects review*
- 2. **Coral Gables Mediterranean Bonus Level 1 and Level 2:**
  - Bonus up to 25% increase of density.
  - Approval needed from the Board of Architects, per Section 5-201, *Coral Gables Mediterranean Style Design Standards*.

## STAFF RECOMMENDATION

### 1. Planned Area Development (PAD):

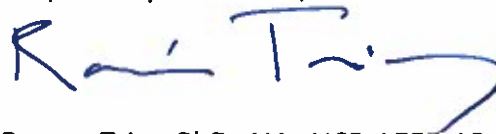
Staff recommends **Approval**.

### 2. Coral Gables Mediterranean Bonus Level 1 and Level 2:

As enumerated in the Findings of Fact contained herein, Staff finds the Application is in compliance with with the Level 1 and Level 2 bonus, per Section 5-200, bonuses are available up to a maximum of 25% increase of density.

Staff recommends **Approval** of the Mediterranean Bonus Level 1 and Level 2.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read 'R. Trias', with a stylized flourish at the end.

Ramon Trias, PhD, AIA, AICP, LEED AP  
Assistant Director of Development Services for Planning  
City of Coral Gables, Florida