



Contoneaster



Contoneaster



Foxtail Ferns



Coccothrinax alta Palms



Sky Vine



Yellow Elder espalier on column



Bougainvillea espalier on columns

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES, FL 33134

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DESIGNS, INC.
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305.774.0022

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NOTE

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SCALE

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PAGE NUMBER

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GABLES STATION

L 1.06



Medjool Date Palm



Seagrape Tree



Montgomery Palm



Clusia rosea Tree



Blue Lantan Palm

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Yellow Tabebuia Tree



Orange Geiger Tree



Floss Silk Tree



Queen Crape Myrtle Tree



Royal Poinciana Tree

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Mahogany Tree



Live Oak Tree



Sabal Palm



Gumbo Limbo Tree



Paradise Tree



Slash Pine Tree

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Jamaican Caper



Simpsons Stopper



Silver Buttonwood



Wild Coffee



Key Thatch Palm



Florida Thatch Palm



Coontie

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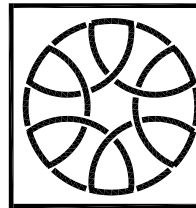
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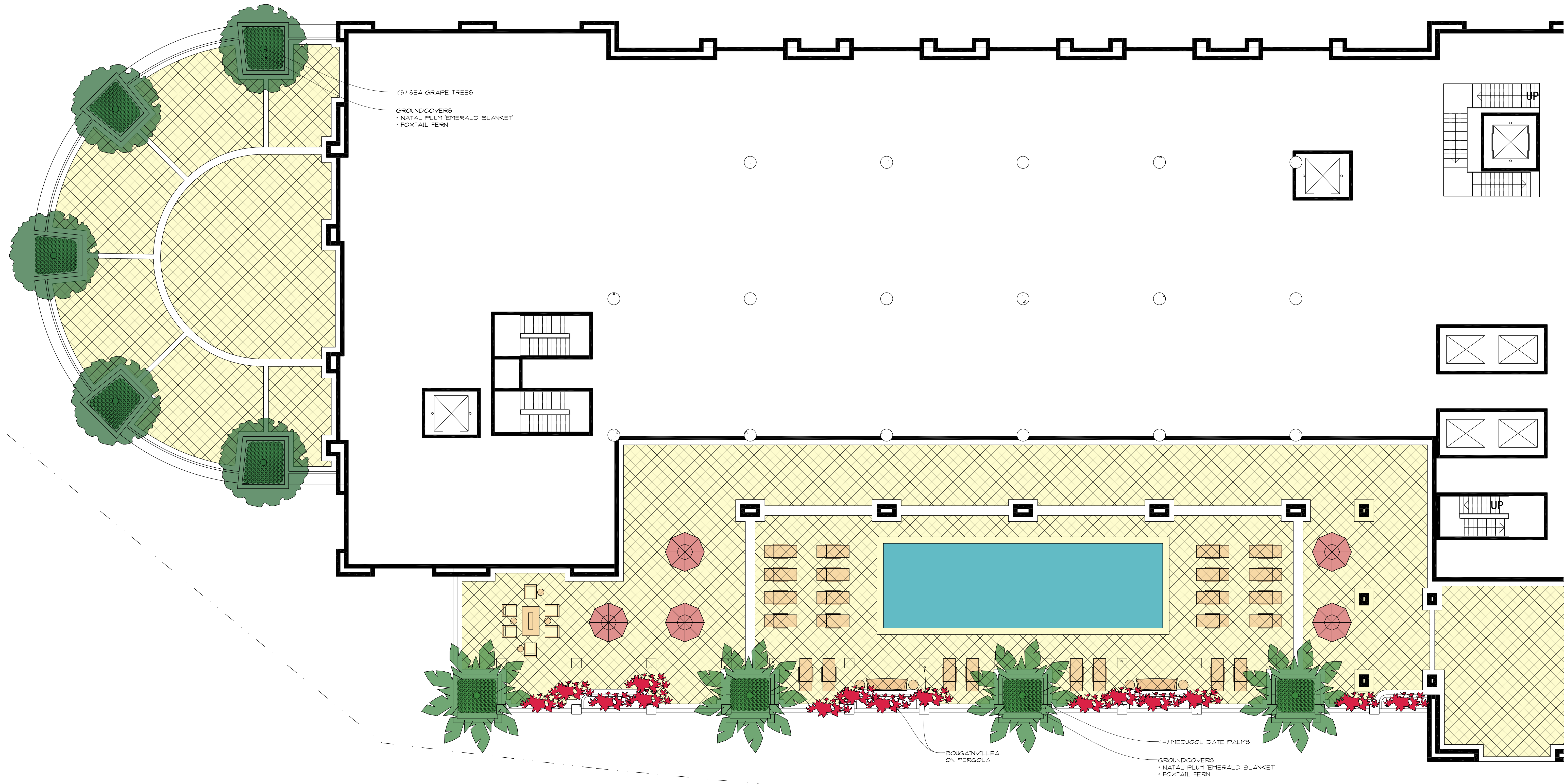
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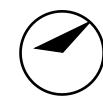
PAGE NUMBER

BUILDING A LEVEL 02



BUILDING A LEVEL 02 - HOTEL POOL TERRACE PLAN

SCALE: 1"=10'-0"



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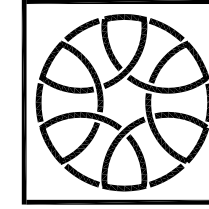
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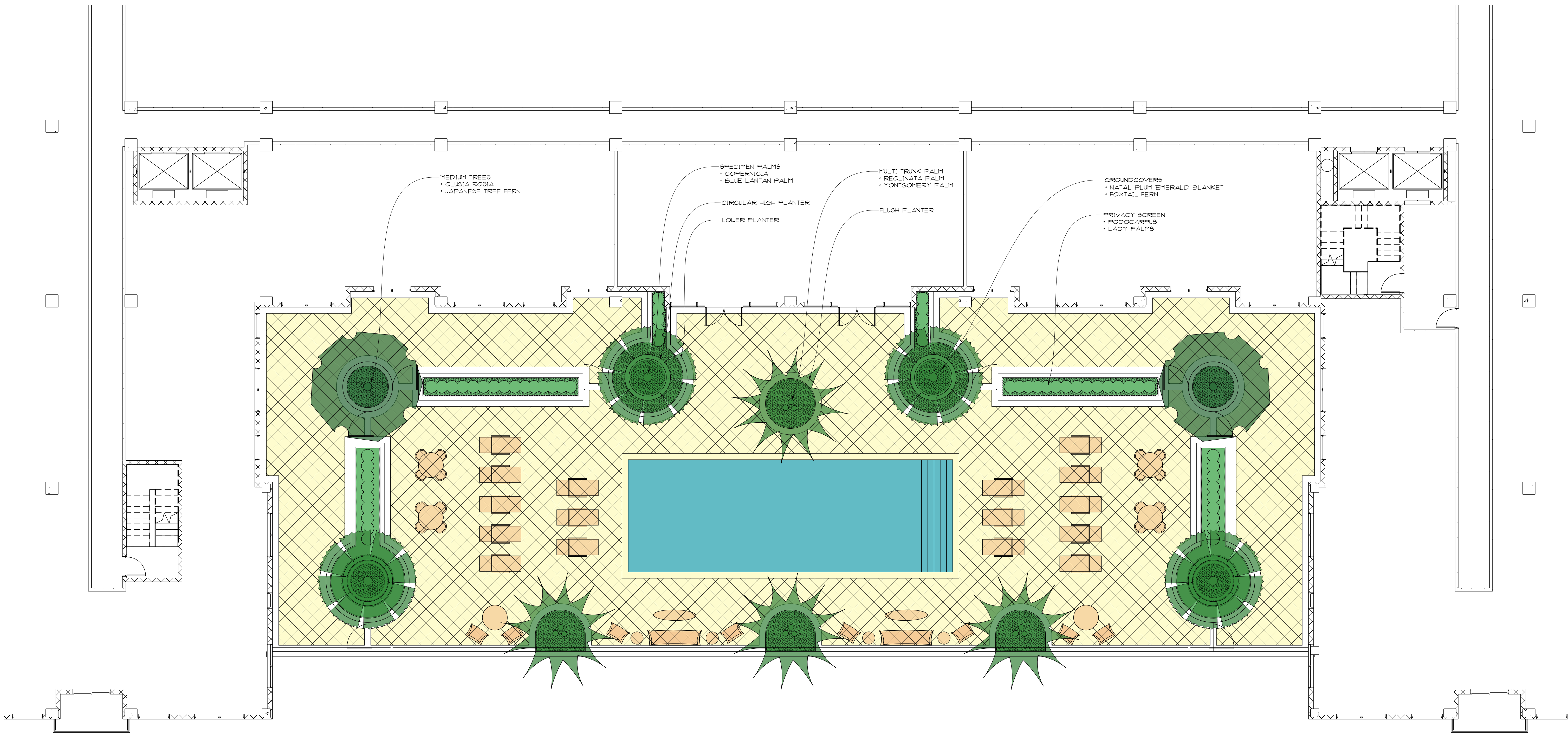
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DRAWING

L 2.00

PAGE NUMBER

BUILDING B LEVEL 07



BUILDING B LEVEL 07 - RESIDENCE POOL TERRACE PLAN

SCALE: 1"=10'-0"

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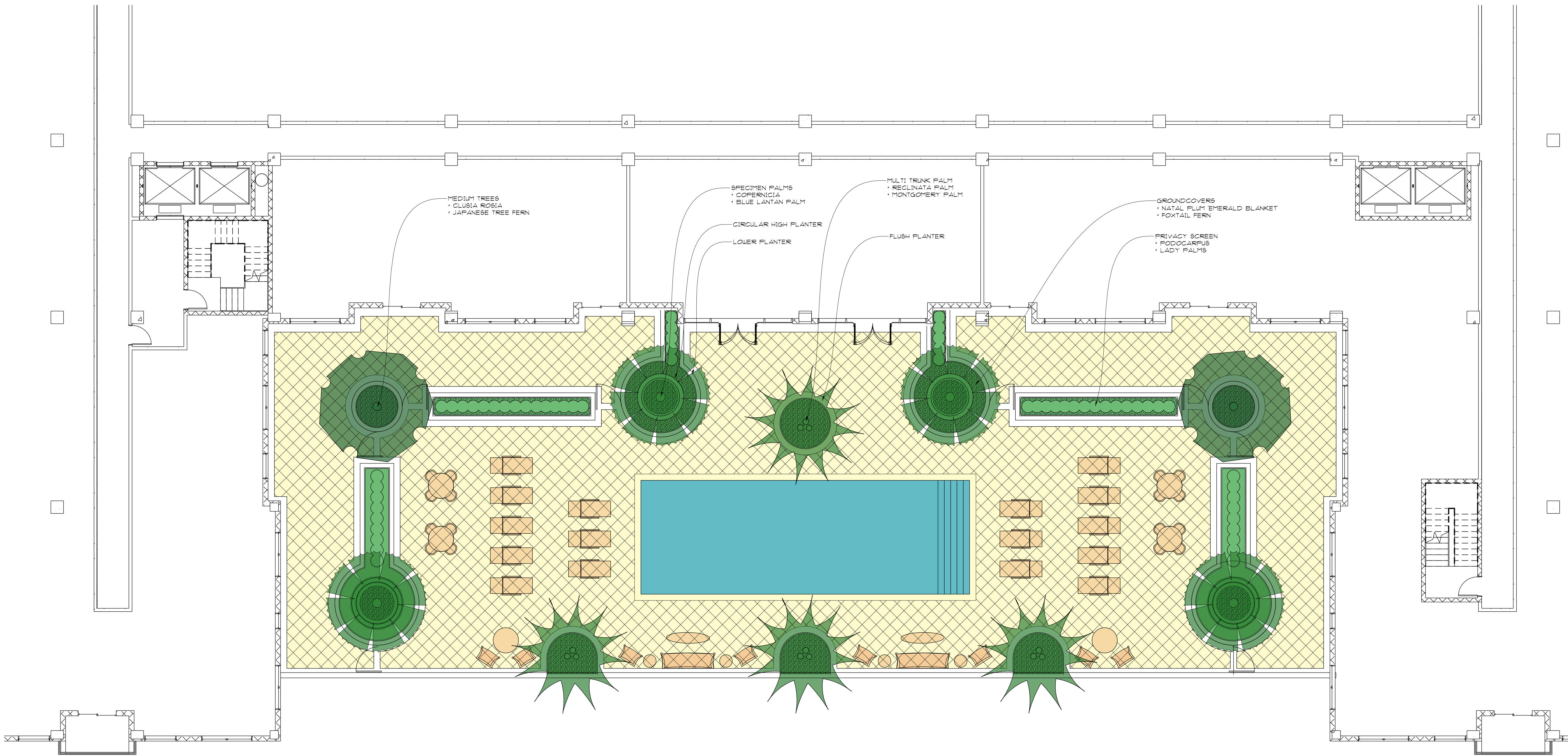
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GABLES STATION

1"=10'-0"

L 7.00

BUILDING C LEVEL 07



BUILDING C LEVEL 07 - RESIDENCE POOL TERRACE PLAN

SCALE: 1"=10'-0"

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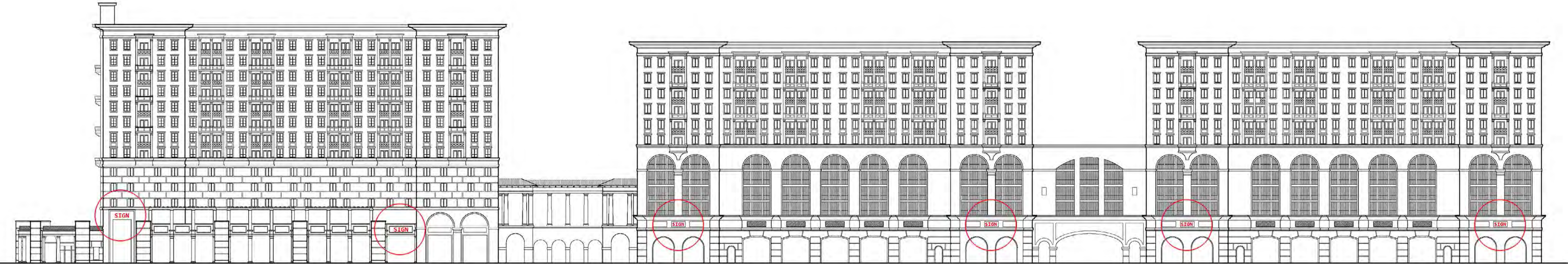
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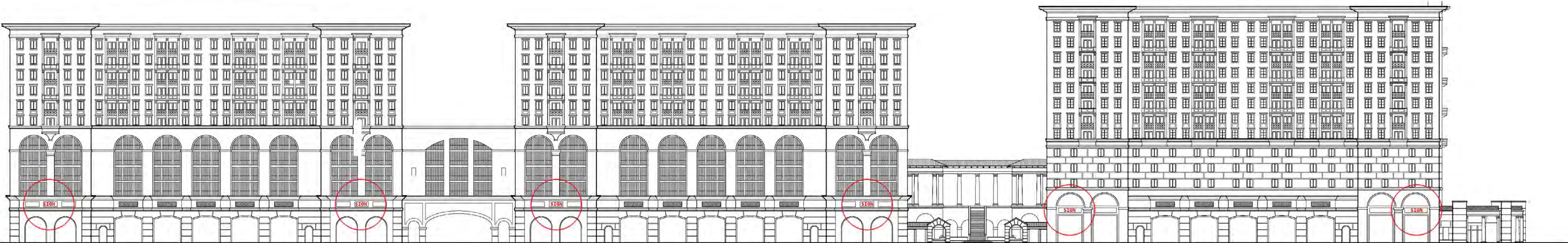
SECTION 05

GENERAL, GRAPHICS, LIGHTING & MATERIALS

SIGNAGE ELEVATIONS



EAST ELEVATION SIGNAGE LOCATIONS



WEST ELEVATION SIGNAGE LOCATIONS

SIGN 18"



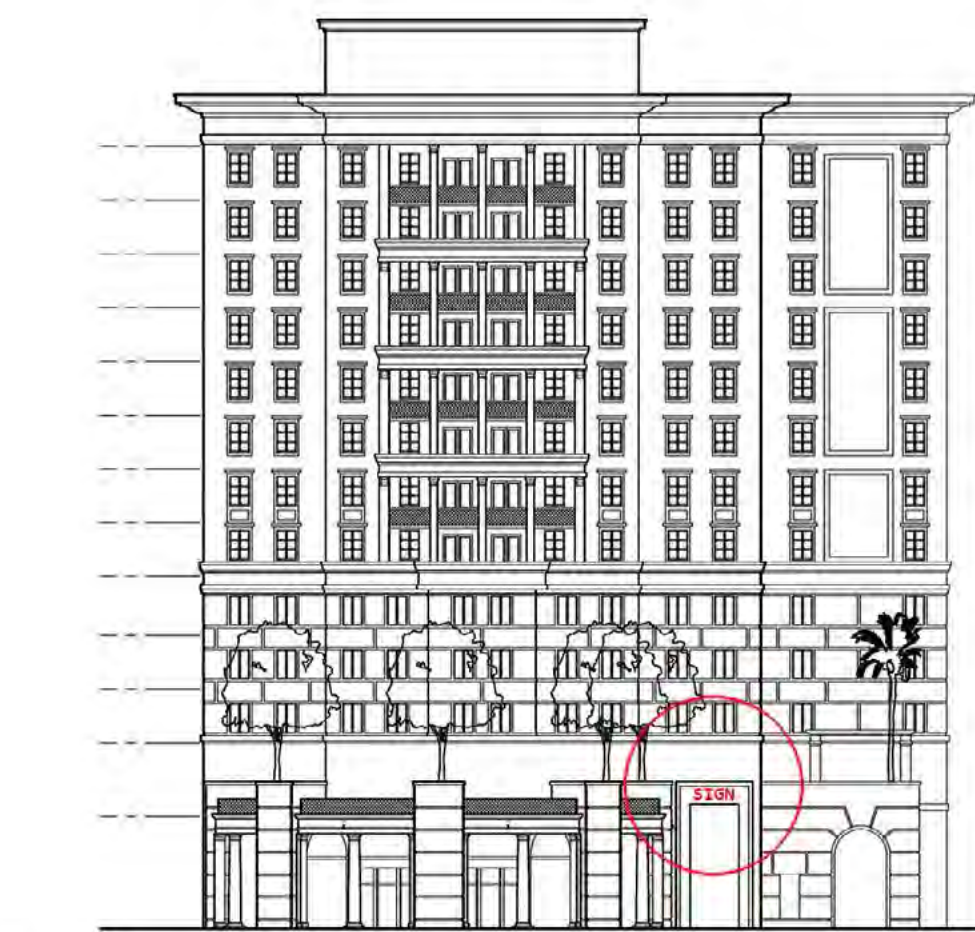
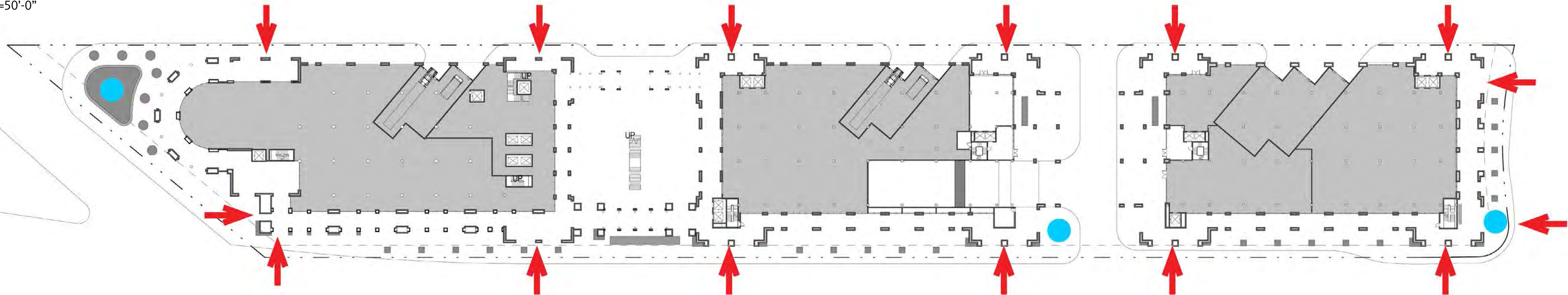
MONUMENT SIGN LOCATION



TENANT SIGN LOCATION

SIGNAGE LOCATIONS KEY PLAN

SCALE: 1"=50'-0"



SOUTH ELEVATION SIGNAGE LOCATIONS



NORTH ELEVATION SIGNAGE LOCATIONS

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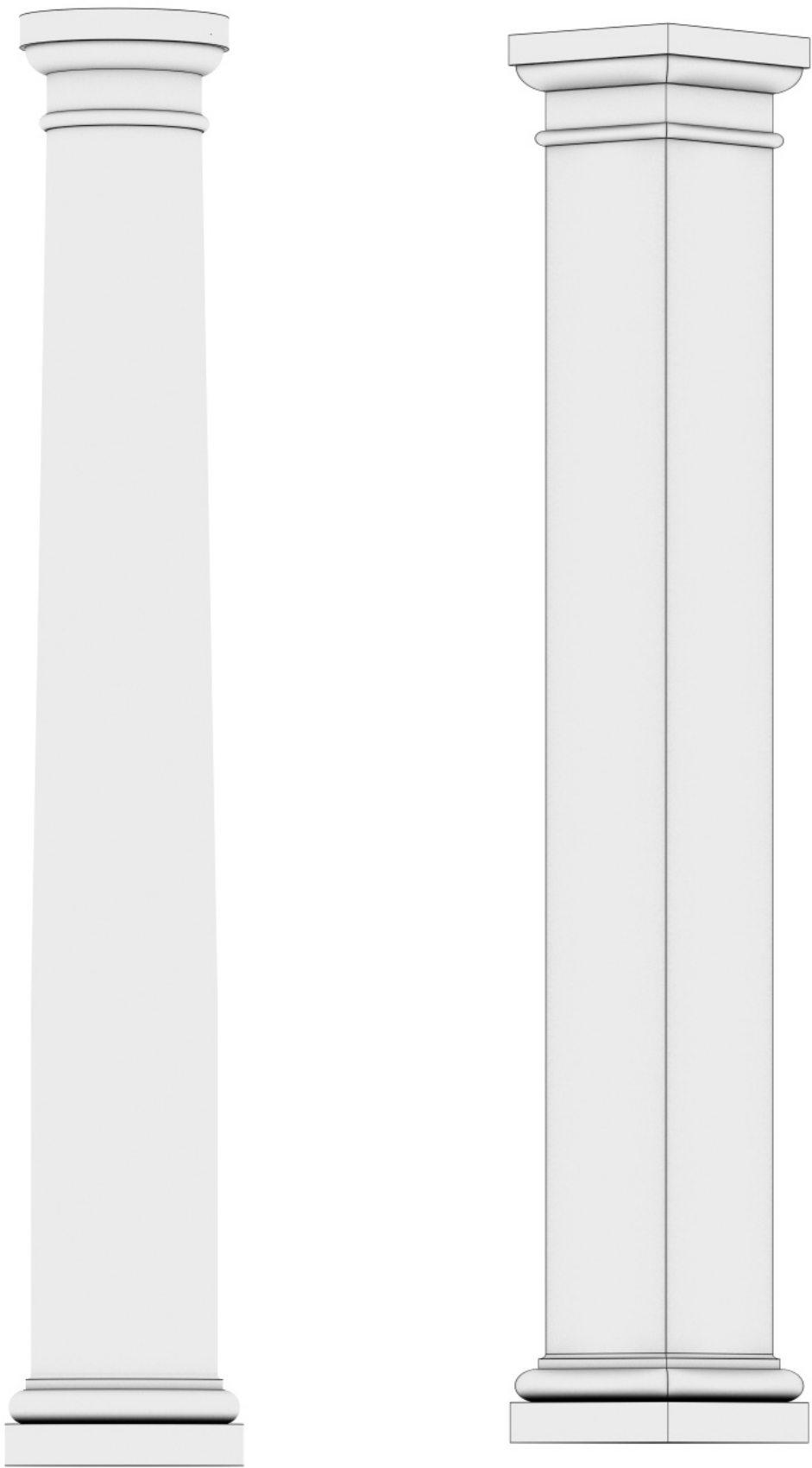
PROJECT NAME GABLES STATION

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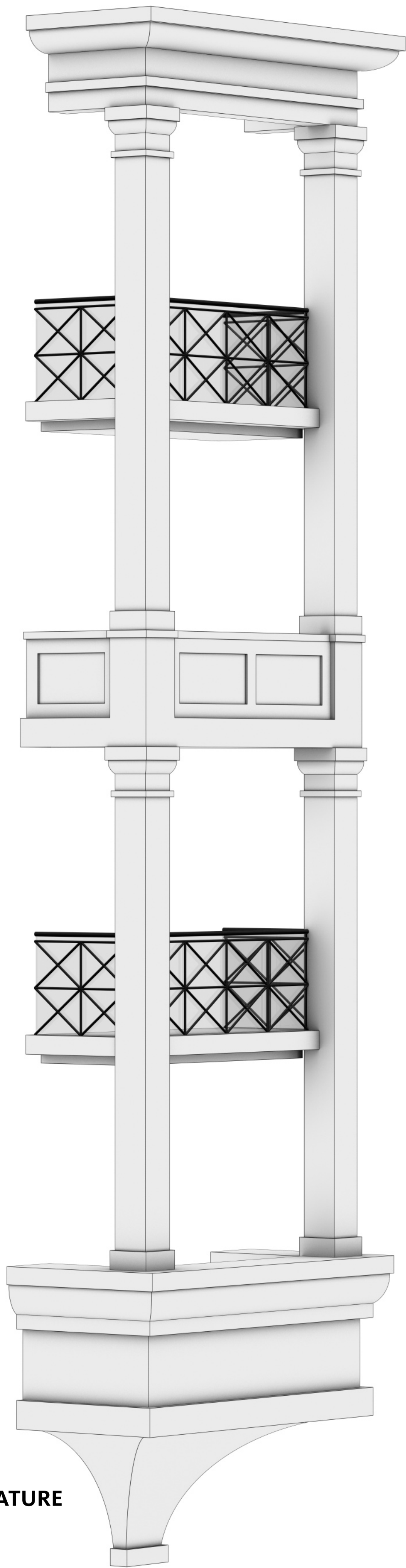
ARCHITECTURAL ELEMENTS



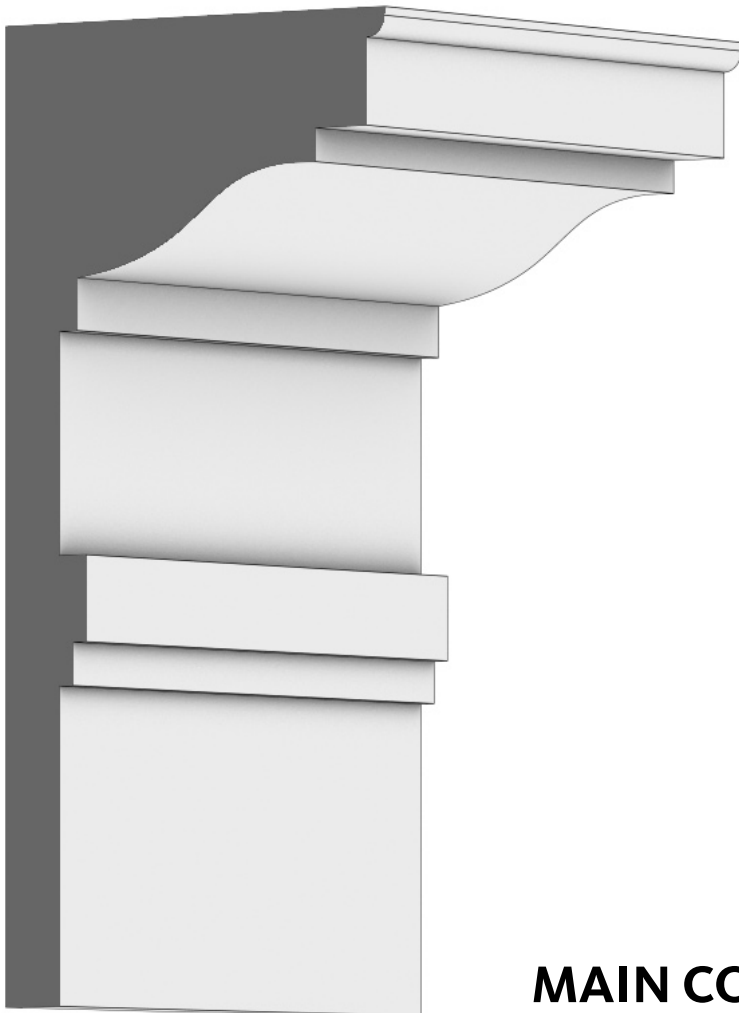
COLUMNS



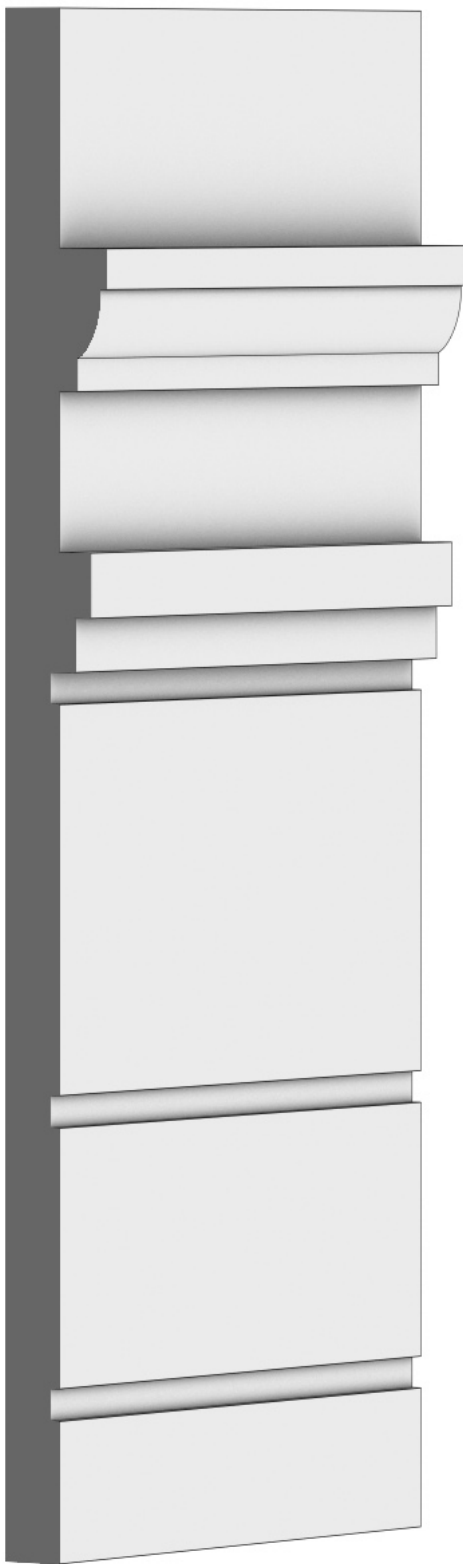
CROWN MOLDING AT BASE



BALCONY FEATURE



MAIN CORNICE



STRING MOLDING

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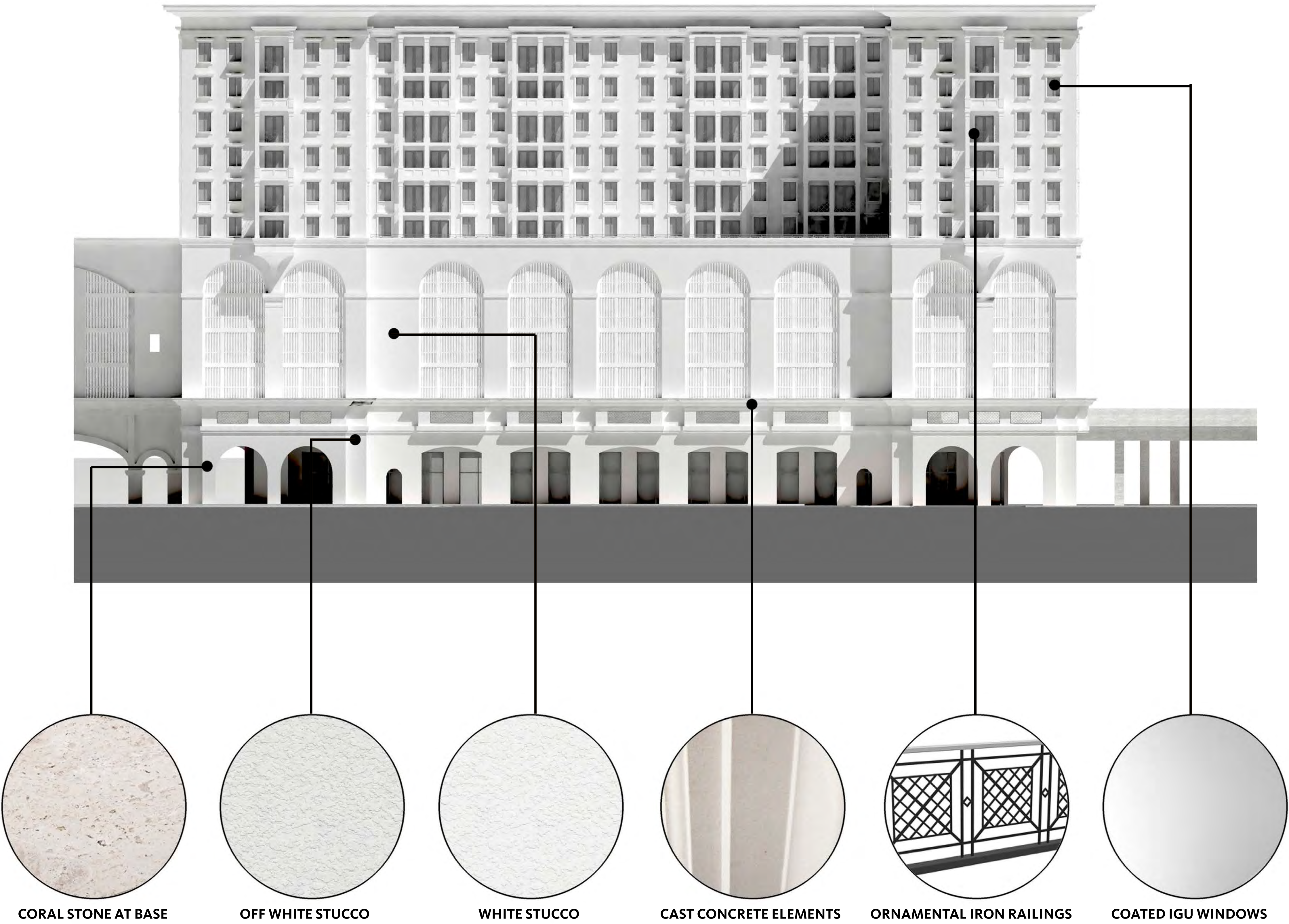
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MATERIALS



CORAL STONE AT BASE

OFF WHITE STUCCO

WHITE STUCCO

CAST CONCRETE ELEMENTS

ORNAMENTAL IRON RAILINGS

COATED IGU WINDOWS

JOB NAME AND LOCATION

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3D MODEL VIEW



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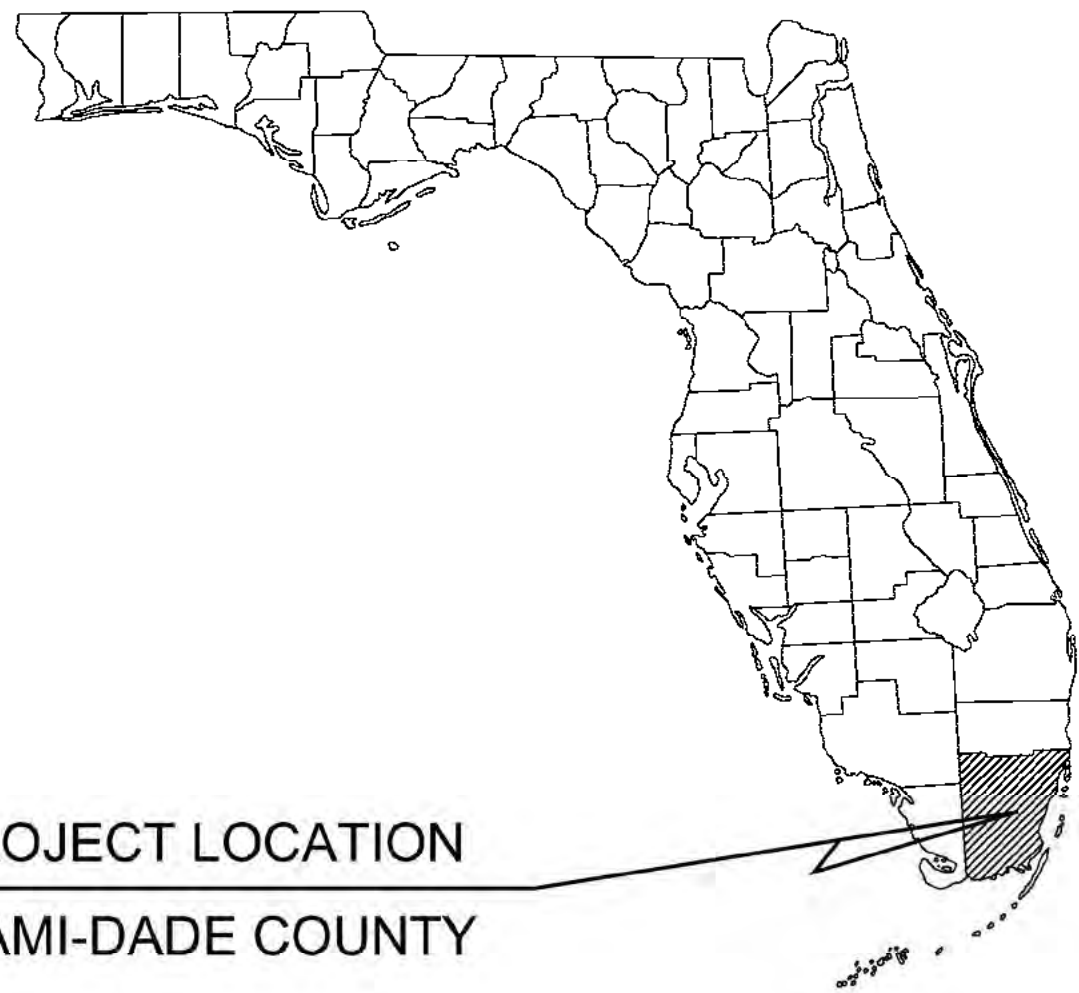
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G 1.05

SECTION 05

CIVIL PLANS



PROJECT LOCATION
MIAMI-DADE COUNTY

PROJECT TEAM:

OWNER/DEVELOPER
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PROJECT LOCATION



LOCATION MAP
N.T.S.



CONCEPTUAL PLANS
FOR
GABLES STATION
PROJECT LOCATED IN
CORAL GABLES, FLORIDA, 33146

Sheet List Table	
Sheet Number	Sheet Title
CS	COVER SHEET
C-001	CONCEPTUAL UTILITIES PLAN KEYMAP
C-002	CONCEPTUAL UTILITIES PLAN
C-003	CONCEPTUAL UTILITIES PLAN
C-004	CONCEPTUAL UTILITIES PLAN
C-005	SIGNAGE AND MARKING KEYMAP
C-006	SIGNAGE AND MARKING PLAN
C-007	SIGNAGE AND MARKING PLAN
C-008	SIGNAGE AND MARKING PLAN
C-009	PUBLIC TRANSPORTATION EXHIBIT

LICENSED PROFESSIONAL
JULIO A. COLLIER, P.E.
FL LICENSE NUMBER
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DATE DESCRIPTION
07.14.2016 City Commission Submittal

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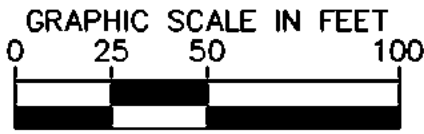
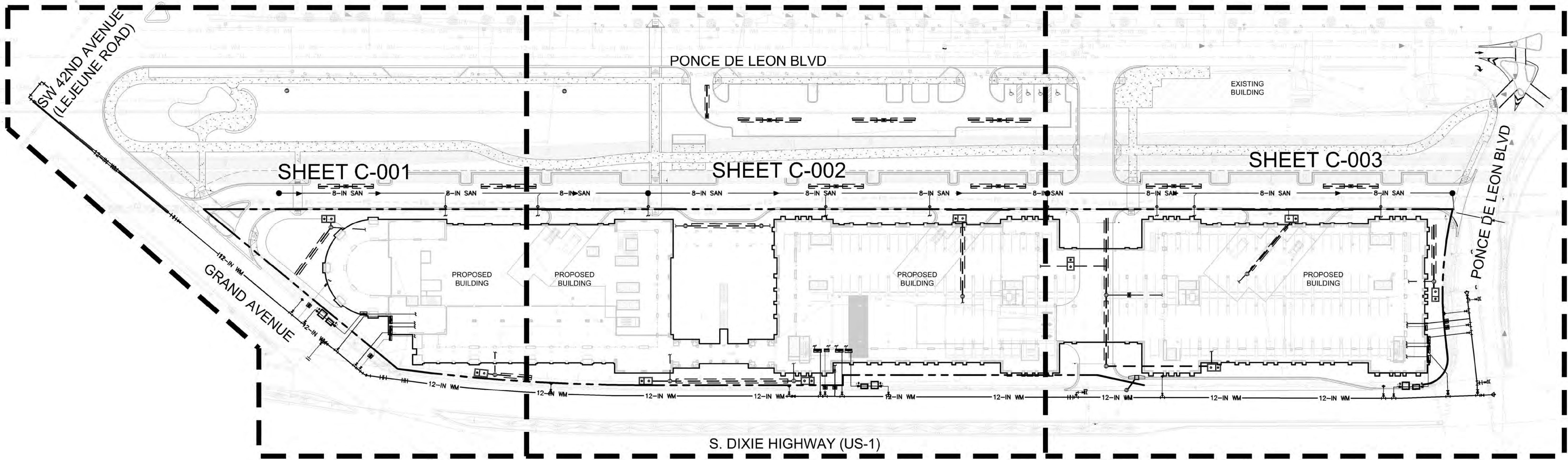
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PROJECT NAME GABLES STATION

SCALE

DRAWING **CS 00**
PAGE NUMBER

CONCEPTUAL UTILITIES PLAN KEYMAP



JOB NAME AND LOCATION

GABLES STATION

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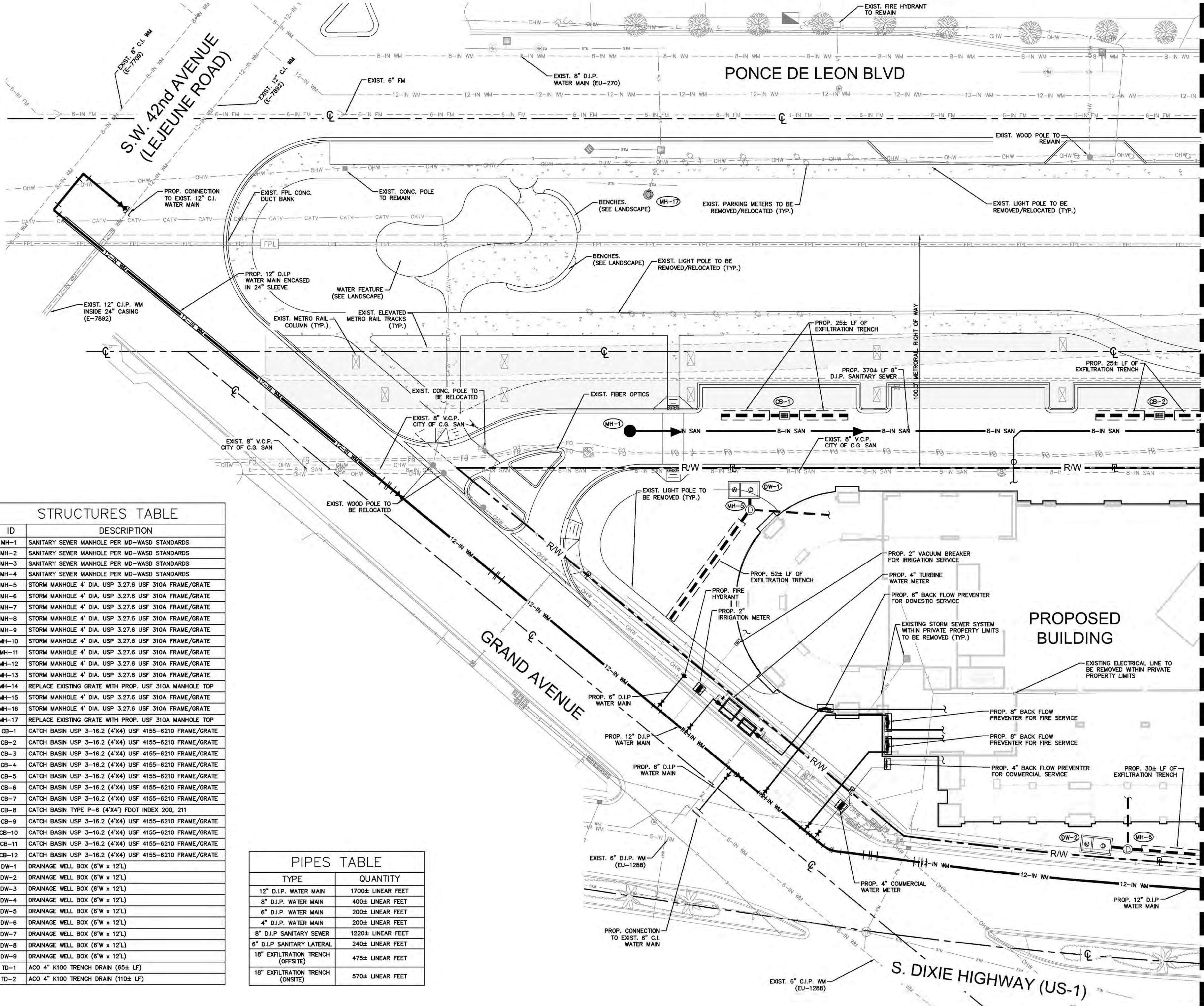
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JULIO A. COLLIER, P.E.
FL LICENSE NUMBER
72489

DATE:

C 001

87

CONCEPTUAL UTILITIES PLAN



- LEGEND
- RAW - RIGHT OF WAY / PROPERTY LINE
 - C - CENTER LINE OF ROADWAY
 - X-IN WM - PROPOSED WATER MAIN
 - X-IN SAN - PROPOSED SANITARY SEWER
 - PROPOSED SANITARY LATERAL
 - PROPOSED STORM SEWER
 - PROPOSED EXFILTRATION TRENCH
 - PROPOSED DRAINAGE WELL
 - OHW - EXISTING OVERHEAD WIRE
 - GAS - EXISTING GAS LINE
 - X-IN FM - EXISTING SANITARY FORCE MAIN
 - X-IN WM - EXISTING WATER MAIN
 - STM - EXISTING STORM SEWER LINE
 - X-IN SAN - EXISTING SANITARY SEWER
 - FO - EXIST. FIBER OPTIC LINE
 - E - EXIST. FPL DUCT AND MANHOLE
 - SMH - EXIST. SANITARY SEWER MANHOLE
 - SSM - EXIST. STORM SEWER MANHOLE
 - SSCB - EXIST. STORM SEWER CATCH BASIN

STRUCTURES TABLE	
ID	DESCRIPTION
MH-1	SANITARY SEWER MANHOLE PER MD-WASD STANDARDS
MH-2	SANITARY SEWER MANHOLE PER MD-WASD STANDARDS
MH-3	SANITARY SEWER MANHOLE PER MD-WASD STANDARDS
MH-4	SANITARY SEWER MANHOLE PER MD-WASD STANDARDS
MH-5	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-6	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-7	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-8	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-9	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-10	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-11	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-12	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-13	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-14	REPLACE EXISTING GRATE WITH PROP. USF 310A MANHOLE TOP
MH-15	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-16	STORM MANHOLE 4' DIA. USP 3.27.6 USF 310A FRAME/GRATE
MH-17	REPLACE EXISTING GRATE WITH PROP. USF 310A MANHOLE TOP
CB-1	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-2	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-3	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-4	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-5	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-6	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-7	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-8	CATCH BASIN TYPE P-6 (4'X4') FDOT INDEX 200, 211
CB-9	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-10	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-11	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
CB-12	CATCH BASIN USP 3-16.2 (4'X4) USF 4155-6210 FRAME/GRATE
DW-1	DRAINAGE WELL BOX (6'W x 12'L)
DW-2	DRAINAGE WELL BOX (6'W x 12'L)
DW-3	DRAINAGE WELL BOX (6'W x 12'L)
DW-4	DRAINAGE WELL BOX (6'W x 12'L)
DW-5	DRAINAGE WELL BOX (6'W x 12'L)
DW-6	DRAINAGE WELL BOX (6'W x 12'L)
DW-7	DRAINAGE WELL BOX (6'W x 12'L)
DW-8	DRAINAGE WELL BOX (6'W x 12'L)
DW-9	DRAINAGE WELL BOX (6'W x 12'L)
TD-1	ACO 4" K100 TRENCH DRAIN (65± LF)
TD-2	ACO 4" K100 TRENCH DRAIN (110± LF)

PIPES TABLE	
TYPE	QUANTITY
12" D.I.P. WATER MAIN	1700± LINEAR FEET
8" D.I.P. WATER MAIN	400± LINEAR FEET
6" D.I.P. WATER MAIN	200± LINEAR FEET
4" D.I.P. WATER MAIN	200± LINEAR FEET
8" D.I.P. SANITARY SEWER	1220± LINEAR FEET
6" D.I.P. SANITARY LATERAL	240± LINEAR FEET
18" EXFILTRATION TRENCH (OFFSITE)	475± LINEAR FEET
18" EXFILTRATION TRENCH (ONSITE)	570± LINEAR FEET

JOB NAME AND LOCATION

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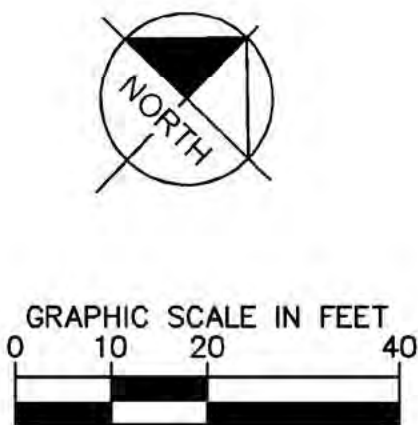
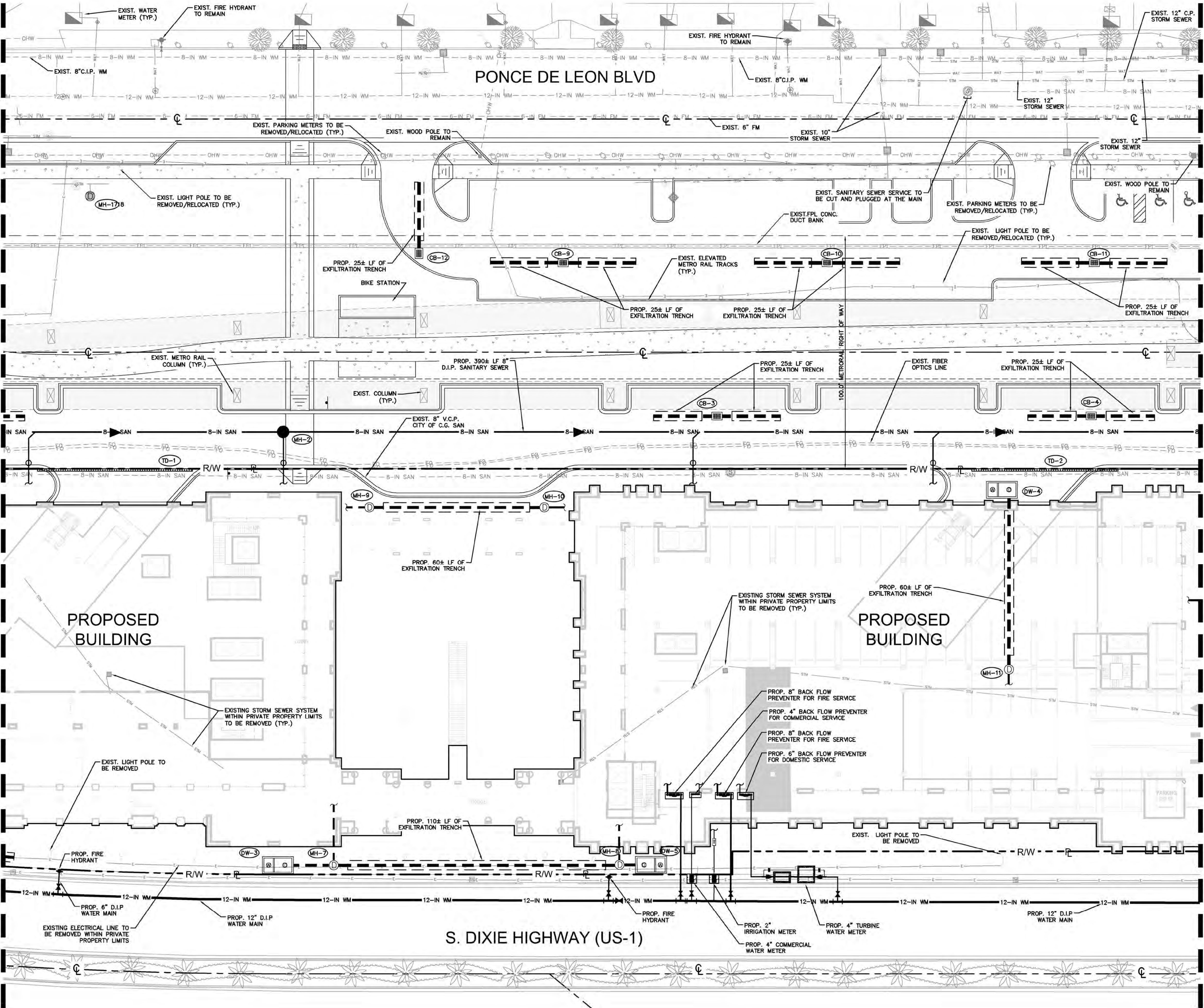
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C 002

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CONCEPTUAL UTILITIES PLAN

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- LEGEND**
- R/W — RIGHT OF WAY / PROPERTY LINE
 - CL — CENTER LINE OF ROADWAY
 - X-IN WM — PROPOSED WATER MAIN
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 - — PROPOSED SANITARY LATERAL
 - — PROPOSED STORM SEWER
 - — PROPOSED EXFILTRATION TRENCH
 - — PROPOSED DRAINAGE WELL
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 - GAS — EXISTING GAS LINE
 - X-IN FM — EXISTING SANITARY FORCE MAIN
 - X-IN WM — EXISTING WATER MAIN
 - STM — EXISTING STORM SEWER LINE
 - X-IN SAN — EXISTING SANITARY SEWER
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 - E — EXIST. FPL DUCT AND MANHOLE
 - — EXIST. SANITARY SEWER MANHOLE
 - — EXIST. STORM SEWER MANHOLE
 - — EXIST. STORM SEWER CATCH BASIN

JOB NAME AND LOCATION
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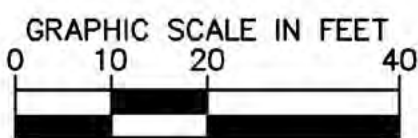
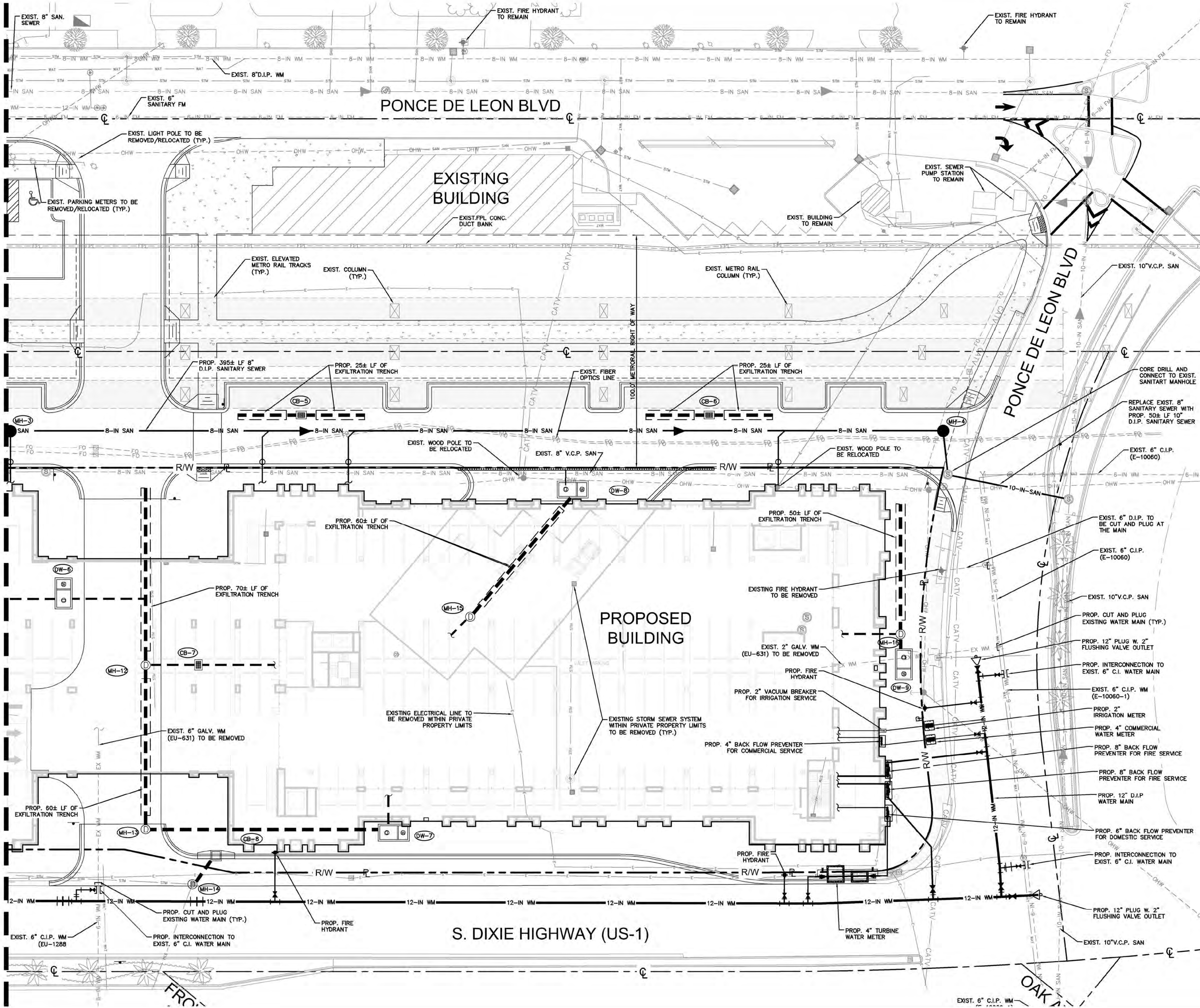
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FL LICENSE NUMBER 72489
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CONCEPTUAL UTILITIES PLAN

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Plotted By: Lopez, Jose
Sheet Set: GABLES STATION
Layout: C-004 CONCEPTUAL UTILITIES PLAN
July 13, 2016 03:16:19pm

SEE SHEET C-003



- LEGEND**
- RAW --- R/W --- RIGHT OF WAY / PROPERTY LINE
 - C --- CENTER LINE OF ROADWAY
 - X-IN WM --- PROPOSED WATER MAIN
 - X-IN SAN --- PROPOSED SANITARY SEWER
 - S --- PROPOSED SANITARY LATERAL
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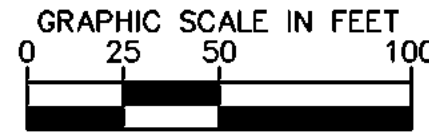
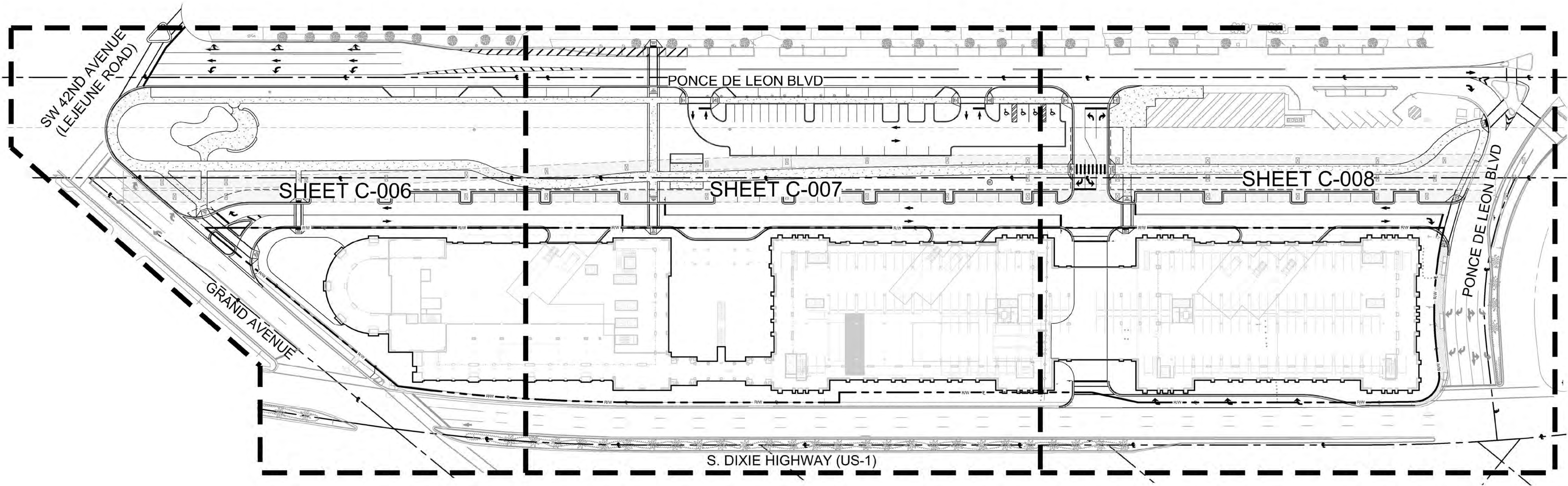
LICENSED PROFESSIONAL
JULIO A. COLLIER, P.E.
FL LICENSE NUMBER
72489

DATE:

C 004

90

SIGNAGE AND MARKING KEYMAP



JOB NAME AND LOCATION
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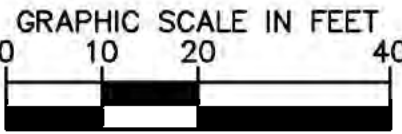
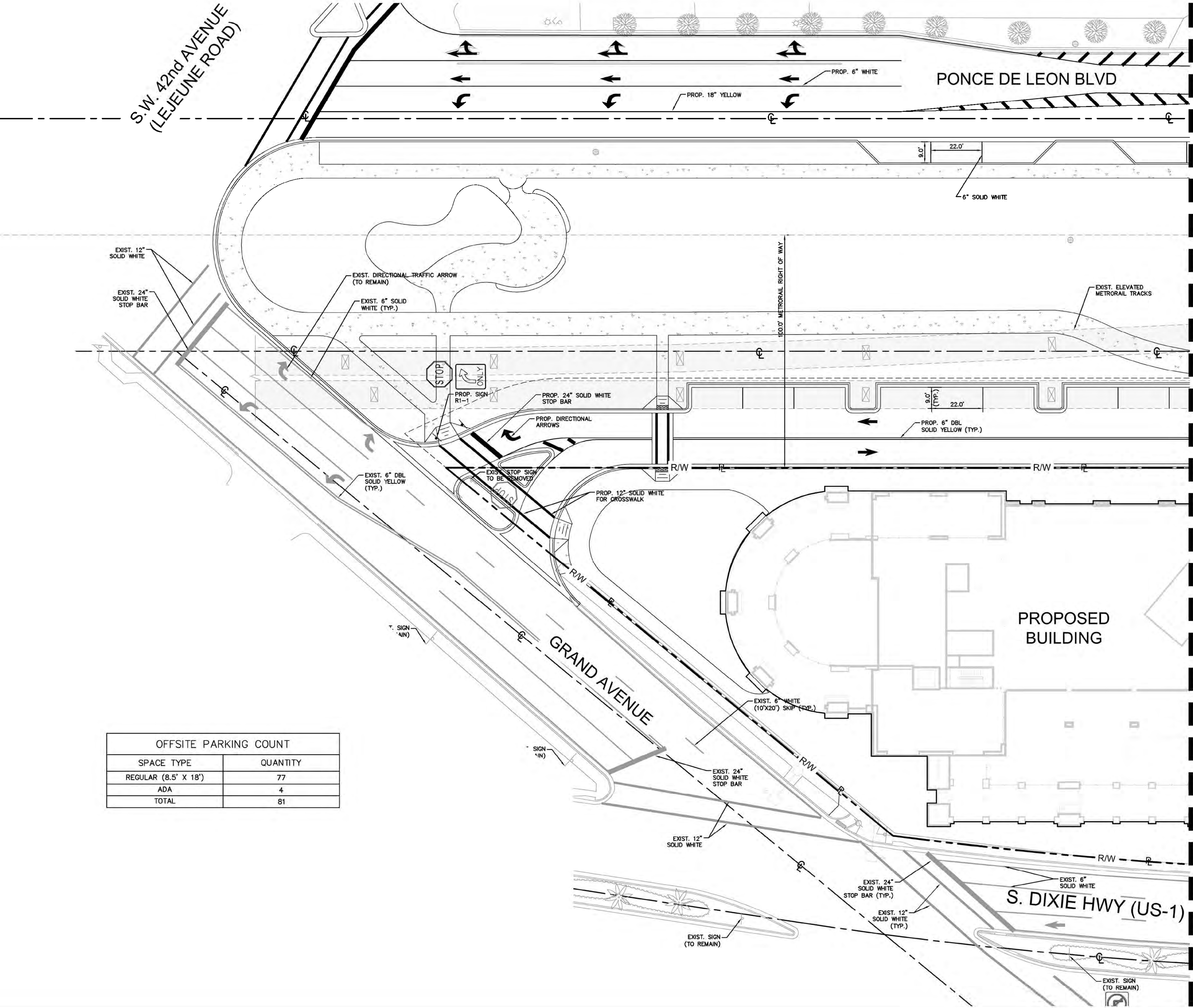
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JULIO A. COLLIER, P.E.
FL LICENSE NUMBER
72489

DATE:

C 005

91

SIGNAGE AND MARKING PLAN



- LEGEND**
- R/W — RIGHT OF WAY / PROPERTY LINE
 - CL — CENTER LINE OF ROADWAY
 - → → PROP. DIRECTIONAL TRAFFIC ARROW PER FDOT INDEX #17346 (TYP.)
 - ==== PROP. 6" DOUBLE YELLOW
 - ==== PROP. PEDESTRIAN CROSSWALK

OFFSITE PARKING COUNT	
SPACE TYPE	QUANTITY
REGULAR (8.5' X 18')	77
ADA	4
TOTAL	81

SEE SHEET C-007

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES, FL 33134

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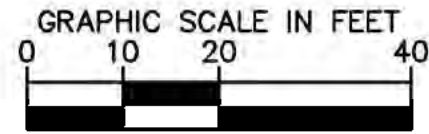
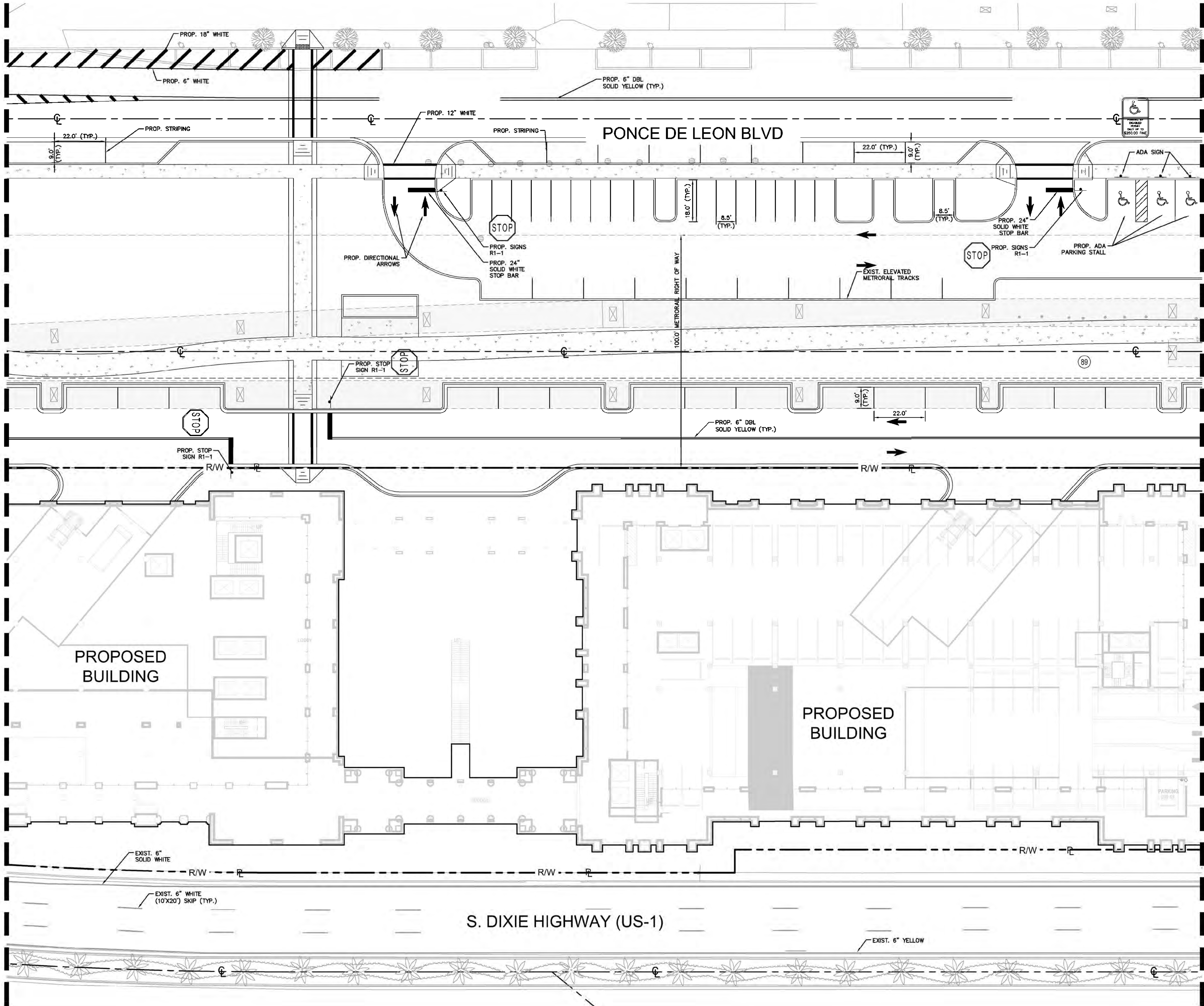
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SIGNAGE AND MARKING PLAN

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SEE SHEET C-006

SEE SHEET C-008



- LEGEND**
- R/W — P — RIGHT OF WAY / PROPERTY LINE
 - C — CENTER LINE OF ROADWAY
 - → → PROP. DIRECTIONAL TRAFFIC ARROW PER FDOT INDEX #17346 (TYP.)
 - == PROP. 6" DOUBLE YELLOW
 - == PROP. PEDESTRIAN CROSSWALK

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07.14.2016	City Commission Submittal

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NOTE DRAWINGS SCALED FOR 24X36" PRINT

PROJECT NAME GABLES STATION

SCALE

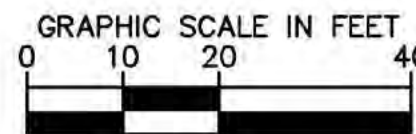
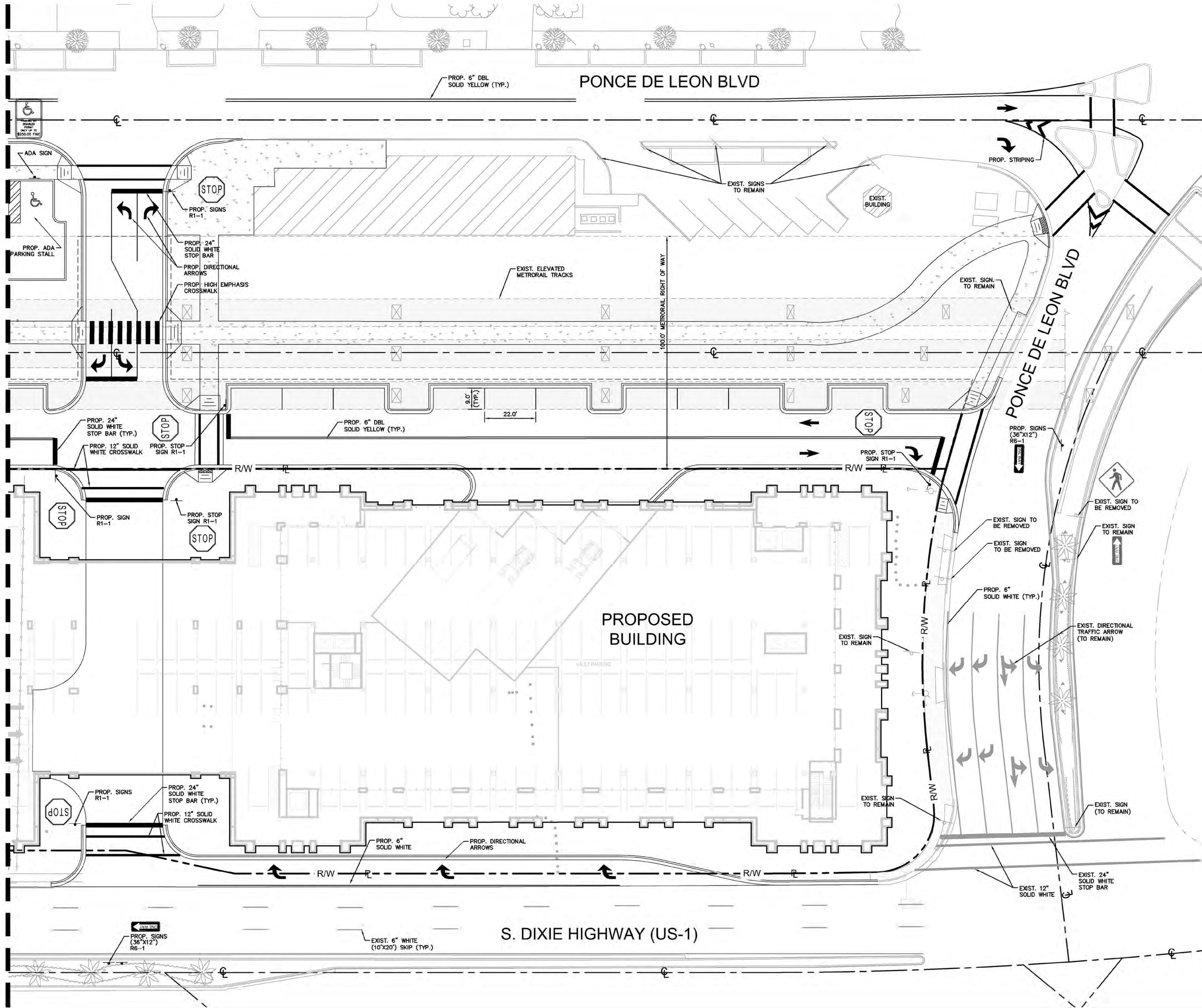
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SIGNAGE AND MARKING PLAN

\\010180 By Lopez, Jose - Sheet Set - Gables Station - Layout C-008 SIGNAGE AND MARKING PLAN July 13, 2016 03:17:34pm K:\ymb-civil\043676000 gables station\Civil\CADD\plan sheets\C-008 SIGNAGE AND MARKING PLAN.dwg

SEE SHEET C-007



- LEGEND**
- R/W — P — RIGHT OF WAY / PROPERTY LINE
 - C — CENTER LINE OF ROADWAY
 - → → PROP. DIRECTIONAL TRAFFIC ARROW PER FDOT INDEX #17346 (TYP.)
 - ==== PROP. 6" DOUBLE YELLOW
 - ==== PROP. PEDESTRIAN CROSSWALK

LICENSED PROFESSIONAL
JULIO A. COLLIER, P.E.
FL LICENSE NUMBER
72489
DATE:

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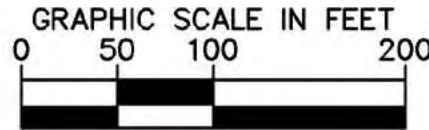
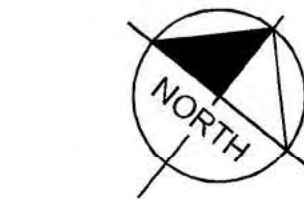
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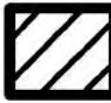
PUBLIC TRANSPORTATION EXHIBIT



MIAMI-DADE COUNTY
BUS STOP



CITY OF CORAL
GABLES TROLLEY STOP



PROJECT LOCAITON

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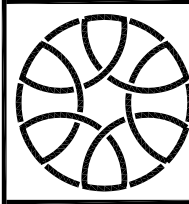
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JULIO A. COLLIER, P.E.

FL LICENSE NUMBER
72489

DATE:

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SECTION 07

HISTORICAL SIGNIFICANCE AND CONCURRENCY STATEMENT

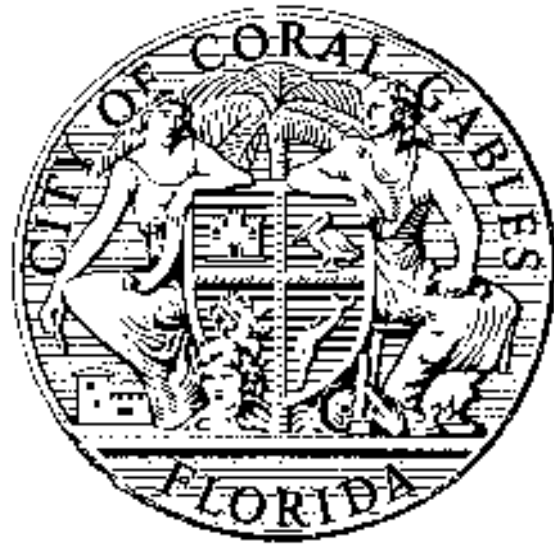
CITY CONCURRENCY IMPACT STATEMENT

CORAL GABLES CONCURRENCY MANAGEMENT

Concurrency Information Statement

This Concurrency Information Statement is for informational purposes only and reflects the availabiltiy of public services only at the time statement is issued.

The available capacity for each public service is monitored and updated as development orders are issued by the city, and the applicant cannot be assured that the necessary public services will be available for a development order (e.g. any change in use) at a future date.



Gables Station
251 S Dixie Hwy
Coral Gables, FL

Multi Family Dwellings: 554 units
Department Store: 79500 Sq.Ft.
Quality Restaurant: 55000 Sq.Ft.
Hotel: 160 rooms
STATUS=P

Date Printed: 1/20/2016
Development Order: 0
Record Number: 3268
Assoc. Demolition Record: 0

Zones:

Trffic	Fire Protection	Flood Protection	Parks and Recreation
23	201	X-500	3

Concurrency Needs

Minimum Required Elevation (ft): 0

Adequate Water Flow for Commercial & Residential Fire Protection

	Site Demand	Zone Capacity	Zone Demand	Concurrent	
Trips	11524			OK	Within Urban Infill Area
Golf Course	0.0923333795	47.41	0.4907901814	OK	
Tennis Courts	0.923333241	40.35	4.9078996372	OK	
Racquetball Courts	0.120495	6.23	0.640554	OK	
BASketball Courts	0.39611	15.34	2.105712	OK	
Ball Diamonds	0.247915	6.27	1.317818	OK	
Playing Fields	0.247915	7.27	1.317818	OK	
Swimming Pools	0.0277	3.13	0.11959	OK	
Equipped Playing Areas	0.277	6.34	1.4729	OK	
Special Recreation Facilities	4.155	93.84	17.935	OK	
Neighborhood Parks (acres)	1.03875	5.62	5.5222	OK	
Mini Parks (acres)	0.0554	0.97	0.29448	OK	
Open Space (acres)	0.1385	1.53	0.7371	OK	
Water Flow (gpm)	3000	3000	3000	OK	

Application Fee: \$190.31
Application Date: 1/20/2016
Expiration Date: N/A

Statement Issued by:

Comments: PROPOSED NEW DEVELOPMENT - MULTI-FAMILY (554) UNITS; SHOPPING CENTER (79,500.0) S.F.;
(2) RESTAURANTS (5500.0) S.F. AND HOTEL (160) ROOMS

Although the purposed use for which this Concurrency Statement is issued is located in the Urban Infill Area of the City of Coral Gables, and the Statement does not reflect the actual trips that would be generated for this use, Concurrency Fees are applicable and will be assessed.

JOB NAME AND LOCATION

GABLES STATION

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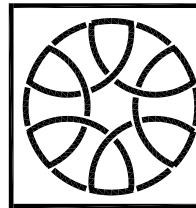
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SCHOOL CONCURRENCY IMPACT STATEMENT



Miami-Dade County Public Schools

giving our students the world

Superintendent of Schools
Alberto M. Carvalho

Miami-Dade County School Board
Perla Tabares Hantman, Chair
Dr. Lawrence S. Feldman, Vice Chair
Dr. Dorothy Bendross-Mindingall
Susie V. Castillo
Dr. Wilbert "Tee" Holloway
Dr. Martin Karp
Lubby Navarro
Dr. Marta Pérez
Raquel A. Regalado

February 12, 2016

VIA ELECTRONIC MAIL

Mario J. Garcia-Serra, Esquire
Gunster
600 Brickell Avenue, Suite 3500
Miami, FL 33131

mgarcia-serra@gunster.com

RE: PUBLIC SCHOOL CONCURRENCY PRELIMINARY ANALYSIS
GABLES STATION
LOCATED AT 251 SOUTH DIXIE HIGHWAY
PH0316020300061 – FOLIO Nos.: 0341200260010 0341200270010, 0341200270020

Dear Applicant:

Pursuant to State Statutes and the Interlocal Agreements for Public School Facility Planning in Miami-Dade County, the above-referenced application was reviewed for compliance with Public School Concurrency. Accordingly, enclosed please find the School District's Preliminary Concurrency Analysis (Schools Planning Level Review).

As noted in the Preliminary Concurrency Analysis (Schools Planning Level Review), the proposed development would yield a maximum residential density of 554 multifamily units, which generate 57 students: 26 elementary, 14 middle, and 17 senior high students. **At this time, all three school levels have sufficient capacity available to serve the application.** However, a final determination of Public School Concurrency and capacity reservation will only be made at the time of approval of final plat, site plan or functional equivalent. As such, this analysis does not constitute a Public School Concurrency approval.

Should you have any questions, please feel free to contact me at 305-995-4501.

Sincerely,

Ivan M. Rodriguez, R.A.
Director

IMR:ir
L-391

Enclosure

cc: Ms. Ana Rijo-Conde, AICP
Mr. Michael A. Levine
Ms. Vivian G. Villaamil
City of Coral Gables
School Concurrency Master File

Planning, Design & Sustainability

Ms. Ana Rijo-Conde, Deputy Chief Facilities & Eco-Sustainability Officer • 1450 N.E. 2nd Ave. • Suite 525 • Miami, FL 33132
305-995-7285 • 305-995-4760 (FAX) • arijo@dadeschools.net



Concurrency Management System (CMS)

Miami Dade County Public Schools

Miami-Dade County Public Schools

Concurrency Management System
Preliminary Concurrency Analysis

MDCPS Application Number: PH0316020300061
Date Application Received: 2/3/2016 11:25:02 AM
Type of Application: Public Hearing
Applicant's Name: Gables Station
Address/Location: 251 South Dixie Highway
Master Folio Number: 0341200260010
Additional Folio Number(s): 0341200270010, 0341200270020,
Local Government (LG): Coral Gables
LG Application Number: (305) 460-5236
Sub Type: Zoning

PROPOSED # OF UNITS 554

SINGLE-FAMILY DETACHED UNITS: 0

SINGLE-FAMILY ATTACHED UNITS: 0

MULTIFAMILY UNITS: 554

CONCURRENCY SERVICE AREA SCHOOLS						
CSA Id	Facility Name	Net Available Capacity	Seats Required	Seats Taken	LOS Met	Source Type
721	GEORGE W CARVER ELEMENTARY	51	26	26	YES	Current CSA
6741	PONCE DE LEON MIDDLE	-52	14	0	NO	Current CSA
6741	PONCE DE LEON MIDDLE	0	14	0	NO	Current CSA Five Year Plan
7071	CORAL GABLES SENIOR	-602	17	0	NO	Current CSA
7071	CORAL GABLES SENIOR	0	17	0	NO	Current CSA Five Year Plan
ADJACENT SERVICE AREA SCHOOLS						
6961	WEST MIAMI MIDDLE	358	14	14	YES	Adjacent CSA
7341	MIAMI JACKSON SENIOR	427	17	17	YES	Adjacent CSA
*An Impact reduction of 22.82% included for charter and magnet schools (Schools of Choice).						

MDCPS has conducted a preliminary public school concurrency review of this application; please see results above. A final determination of public school concurrency and capacity reservation will be made at the time of approval of plat, site plan or functional equivalent. **THIS ANALYSIS DOES NOT CONSTITUTE PUBLIC SCHOOL CONCURRENCY APPROVAL.**

1450 NE 2 Avenue, Room 525, Miami, Florida 33132 / 305-995-7634 / 305-995-4760 fax /
concurrency@dadeschools.net

JOB NAME AND LOCATION

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FL 33134

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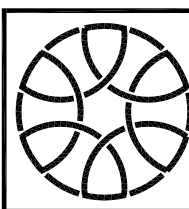
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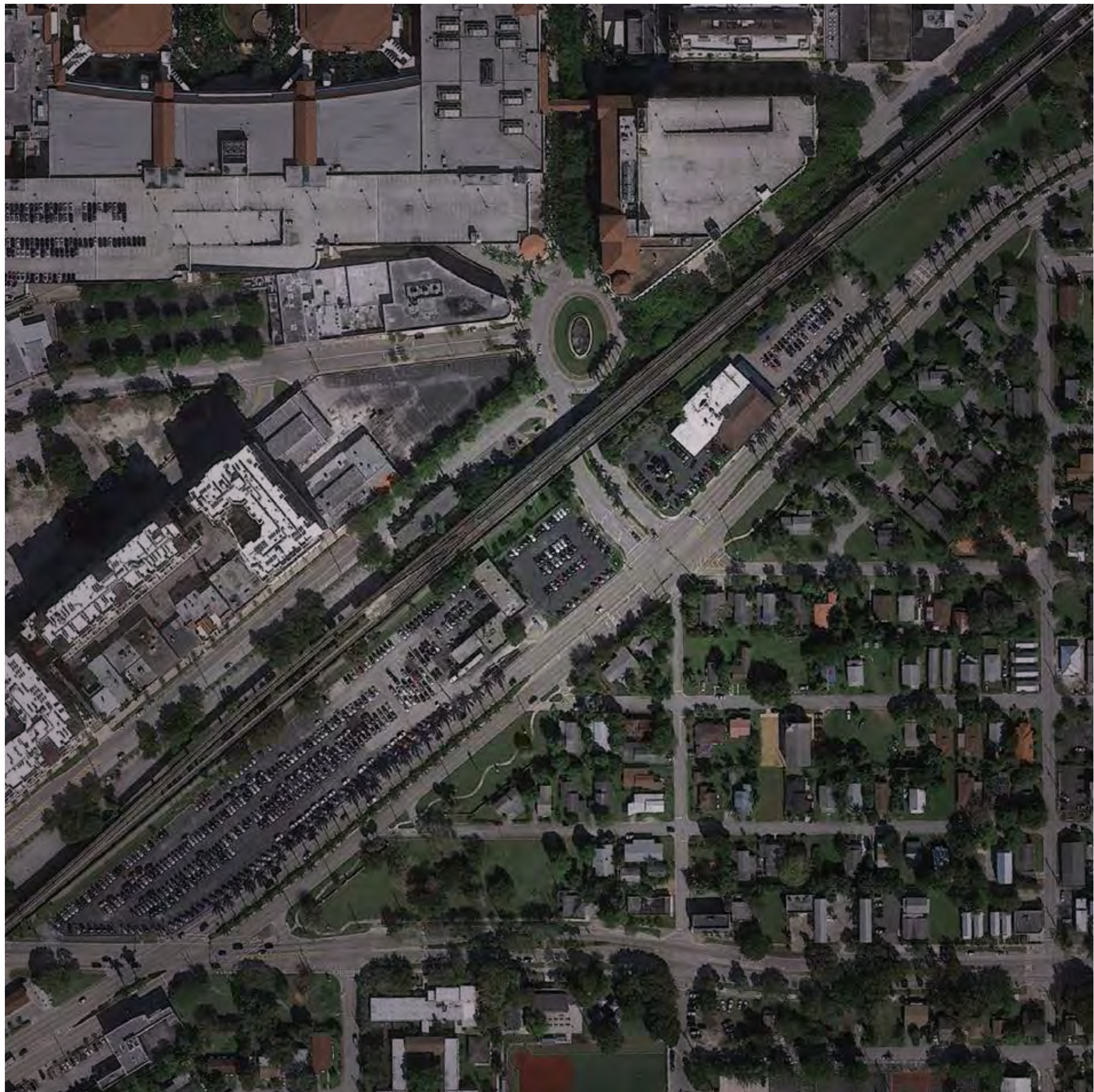
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SEC.07

PAGE NUMBER

SECTION 08

TRAFFIC STUDY



Gables Station
Traffic Impact Study

Responses to City of Coral Gables Public Works Traffic Analysis Comments
(June 15th, 2016)
Re: Review of Gables Station Traffic Impact Analysis Dated April 2016

1. Pages 2, 9, 18, 22, 26, 28 and 29: On the figures on these pages, clarify for the reader what the acronym VMP represents (Village of Merrick Park).

Response: In order to clarify that VMP is Village of Merrick Park, a legend has been added to all exhibits as needed.

2. Page 4: Under “Future Traffic Conditions”, clarify the existing statement to indicate that traffic service was tested for both without and with the project.

Response: Text has been revised.

3. Pages 12, 17 and 24: On Exhibits 4, 6 and 11, in the second column title or in a footer, explain that S, U, and R represent Signalized, Un-signalized, and Roundabout, respectively.

Response: Exhibits 4, 6 and 11 have been updated to explain what S, U, and R represent.

4. Page 14: Text should refer to the historical count station traffic data available in Appendix C.

Response: Text has been added to reference Appendix C.

5. Pages 19-20: Trip Generation:

a. The text states that a 10% adjustment was applied to the trip generation to account for other modes of transportation. Actually, the trip generation table shows a 10% adjustment for transit trips and 10% adjustment for pedestrian trips (from the apartments only). Text should match table calculations.

Response: Text has been revised to match the adjustments shown on Exhibit 8.

b. The methodology statement assumed 554 apt. units, 268 hotel rooms, and 88,000 sf retail. The current proposal is for 460 apt. units, 147 rooms, and 60,000 sf retail. The text reference to the methodology should note the reduction.

Response: Since the submittal of the traffic study, the proposed development plan has been revised. The study has been updated for the current development plan.

JOB NAME AND LOCATION

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PAGE NUMBER	

RESPONSES TO TRAFFIC ANALYSIS COMMENTS

- c. Hotel trips were calculated using “Rooms” instead of “Occupied Rooms” under ITE 310 – total should be 98 in AM and 102 in PM.

Response: Hotel trips were calculated using “Rooms” because data on occupancy rate for the proposed hotel use was not provided. Although, if “Occupied Rooms” is used, then a 100% hotel occupancy (98 AM trips / 102 PM trips) is not realistic. However since the submittal of the traffic study, the proposed development plan has been revised. The study has been updated for the current development plan.

- d. Shopping Center trips calculated using the ITE equation generate 452 PM peak trips, not 425.

Response: Exhibit 8 correctly shows the PM trips (204 entering + 221 exiting = 425 trips) for the 60,000 SF of shopping center based on ITE equations, also shown on the trip generation worksheet included in Appendix F. However since the submittal of the traffic study, the proposed development plan has been revised. The study has been updated for the current development plan.

- e. Pass-By – it is not clear where the 45% pass-by comes from. ITE data shows 34% for Shopping Center in PM, and 0% for AM. Understanding that ITE does not publish pass-by information for AM, the 12% as discussed on page 19 appears acceptable. Revise or explain the 45% value.

Response: Pass-by trips for the shopping center land use are directly related to the size of the shopping center. ITE 3rd Edition Trip Generation Handbook provides a graph (F.7) which plots all actual data points and shows this relationship. Therefore, the overall average pass-by percentage of 34% was not used. In order to represent the proposed size of the shopping center, an average of the data points with shopping centers approximately 60,000 SF in size was calculated. The pass-by trip reduction of 45% taken was based on this average.

- f. The information in Appendix F should be reviewed. Trip generation for ITE 820 used average rate, not equation. Also, note the pass-by credit shown in calculations here is 0% in AM and 34% in PM.

Response: The correct trip generation worksheet has been included in Appendix F. The TripGen software shows pass-by trips based on the average rate (34%) when using rates for the vehicle trips. However when using equations, the software will calculate the pass-by trips using the relationship to shopping center size (mentioned in Comment 5.f) resulting in a 45% pass-by deduction during the PM peak hour.

- g. Appendix F should include actual ITE internal capture matrix diagrams. 18% seems reasonable, but it is difficult to verify.

Response: An internal capture matrix was provided in Appendix F.

6. Page 22: On Exhibit 10 – Project Trip Distribution, please note the following and correct.

- a. The outbound distribution percentages at the outer points of the street network shown total to only 5%. Perhaps 5% approaching from Bird Road east of Ponce de Leon Blvd (WB to SB turn) is missing.

Response: It seems comment 6a is discussing the inbound distribution not the outbound. If this is correct, then the 10% assigned to southbound on Ponce de Leon Blvd at Bird Road should be a 15%. Exhibit 10 has been revised and the total inbound percentage is now 100%.

- b. The inbound distribution percentages at the outer points of the street network shown total to only 90%.

Response: It seems comment 6b is discussing the outbound distribution not the inbound. If this is correct then the total outbound percentage shown on the exhibit is 95% not 90%. Exhibit 10 has been revised to show the missing distribution of the southeast approach at the intersection of Le Jeune Rd / Ponce de Leon Blvd / Grand Ave. The total outbound percentage is now 100%.

- c. Inbound and outbound distribution percentages at the outer points of the street network shown are not balanced. While total symmetry is not mandatory, some explanation of differences would be helpful.

Response: Most of the outer points of the network are balanced. In few instances, 5% was reassigned due to driveway locations, roadways available to travel in the desired direction, and attractiveness of traveling on a specific roadway.

- d. Will queuing from the outbound share of 25% exiting from the site drive to US 1, with LOS F at US 1, in turn block the exit from the garage?

Response: The upstream signal on US-1 will provide gaps for the outbound vehicles at the project driveway. Furthermore, the proposed garage ramp will provide two outbound lanes. Therefore right-turn vehicles are not expected to block the garage exit.

7. Page 25: In Exhibit 11 (Continued), first entry addresses the project driveway connecting to South Ponce de Leon Blvd. near the 4550 block.

- a. As unsignalized, is the driveway intersection analysis based on a two-lane driveway approach (left turn and right turn lanes) and a stop sign only for the driveway?

Response: This driveway was analyzed using a right-turn only lane at the NE approach. This the only stopped controlled approach at this intersection.

- b. It appears the driveway will intersect South Ponce de Leon Blvd. between the exit and entrance to parking lots running along street. How will these parking access/egress points be treated at the driveway intersection?

Response: Since the submittal of the traffic study, the proposed site plan has been revised. The driveway has been moved further east. Also the driveway will be stopped controlled.

8. Page 27: Under Section 5.0 – Circulation Plan:

- a. Will project pedestrian movements be accommodated along the project driveway to South Ponce de Leon Blvd. As there is no sidewalk along the southeast side of this street, will pedestrian crosswalks to the northwest side of the street be provided?

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES, FL 33134

PROPERTY OWNER

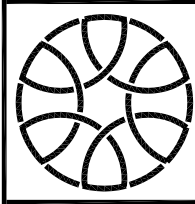
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RESPONSES TO TRAFFIC ANALYSIS COMMENTS

Response: The project is providing sidewalks along the southeast side of Ponce de Leon Blvd and across the driveway. It is also considering a pedestrian crosswalk to the northwest side of Ponce de Leon Boulevard. If proposed, the pedestrian crosswalk needs to be approved by city of Coral Gables and Miami-Dade County.

- b. How will the crossing of the greenway trail along the Metrorail corridor be treated where it crosses the driveway?

Response: The proposed crossing is included in Appendix G.

- c. Are any other pedestrian or bicycle improvements needed for connectivity to sidewalks, paths, or transit stops?

Response: The proposed site plan provides for connectivity to sidewalks, paths, and transit stops.

- d. Verify that site roadway geometry will accommodate the proposed truck loading zones without impeding site circulation and for reasonable truck movements to enter and depart from truck bays.

Response: An Autoturn analysis has been completed and included in Appendix G.

9. Page 29: In several of the intersection approaches there seems to be a discrepancy between the LOS posted in Exhibit 11 and the LOS from the Synchro reports.

Response: Exhibit 11 has been checked and does not have any discrepancy.

10. Page 30: Conclusions – The text states in Paragraph 1 that signal timing improvements are suggested at four locations. What sort of changes are proposed? Would they significantly improve traffic service? Would these changes be acceptable to FDOT and/or Miami-Dade County as applicable, and are they compatible with overall traffic signal progression schemes in place?

Response: Text has been added to the report to clarify the proposed signal timing improvements at each intersection and synchro ‘timing’ reports showing the improvements are included in Appendix D. The minor signal timing improvements show a decrease in approach delay and optimized intersection operations. The recommended signal timing improvements will be sent to the Miami Dade County Traffic Signals and Signs Division. They will usually send a letter back indicating that once the proposed project is open to traffic, a site engineer will observe the intersection during the peak hours. At that point the county will make the signal timing changes as needed.

Please verify therefore that future traffic condition analyses were performed using existing traffic signal timings.

Response: The “Future with Project” conditions are analyzed with existing signal timing. The “Future with Project with Improvement” conditions are analyzed with the proposed signal timing.

11. Appendix A: Please use directional and turn arrows to depict what site access and egress movements are possible with the proposed site plan.

Response: An exhibit showing site circulation has been included in Appendix A.

12. Appendix D: Synchro Analysis - it appears from the Synchro 9 reports that HCM 2000 was used instead of HCM 2010 for several intersections. It is suggested to use the most recent HCM or provide reasoning why it isn’t used.

Response: HCM 2010 was used when synchro allowed. However, for some intersections it does not support the lane configuration/signal timing phasing. Therefore HCM 2000 was used.

13. General: There are various typos which were identified and are included in the attached scanned PDF document.

Response: Typos have been revised accordingly.

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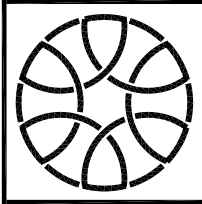
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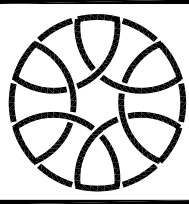
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EXECUTIVE SUMMARY

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EXECUTIVE SUMMARY

The proposed Gables Station project will be located on the west side of US-1 between Ponce de Leon Boulevard and Grand Avenue in Coral Gables, Florida. The site is located within the Gables Re-development Infill District (GRID), the city’s traffic concurrency exception area. It should be noted that the site was previously approved for a project proposing 295,000 GLSF of shopping center. The current proposed development program shows a substantial decrease of project trips generated when compared to the previously approved Gables Station Project. The current development program includes 526 apartment units and 75,294 SF of retail space. Access to and from the project site will be accommodated on East Ponce de Leon Boulevard, South Ponce de Leon Boulevard, US-1, and Grand Avenue.

This traffic study is consistent with the methodology previously discussed with and agreed to by the city of Coral Gables Public Works Department. For the purpose of this traffic study, project buildout is anticipated in 2018.

An assessment of the traffic impacts associated with the proposed project was performed in accordance with the requirements of the City of Coral Gables. The results show that the overall intersections’ LOS is currently and projected to be within the City’s LOS standards; however, minor signal timings during AM and PM peak period are recommended to improve the operations of certain approaches at the following intersections:

- Ponce de Leon Boulevard / LeJeune Road / Grand Avenue – adjust signal timing to provide more green time to the southwest and westbound movements
- LeJeune Road / US-1 – adjust signal timing to provide more green time to the northbound movements
- Grand Avenue / US-1 – adjust signal timing to provide more green time to the eastbound / westbound movements
- Ponce de Leon Boulevard / US-1– adjust signal timing to provide more green time to the southbound movements

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In addition, a mobility and circulation plan was completed as part of the study. The plan shows that the project area is currently served by various Miami-Dade Transit bus routes, the Metrorail, and the city of Coral Gables Trolley. The project is located in an area that is conducive for pedestrian and bicycle activities providing bike paths, ample sidewalks, and crosswalks.

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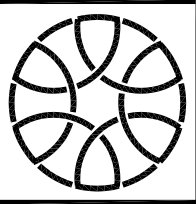
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1.0 INTRODUCTION

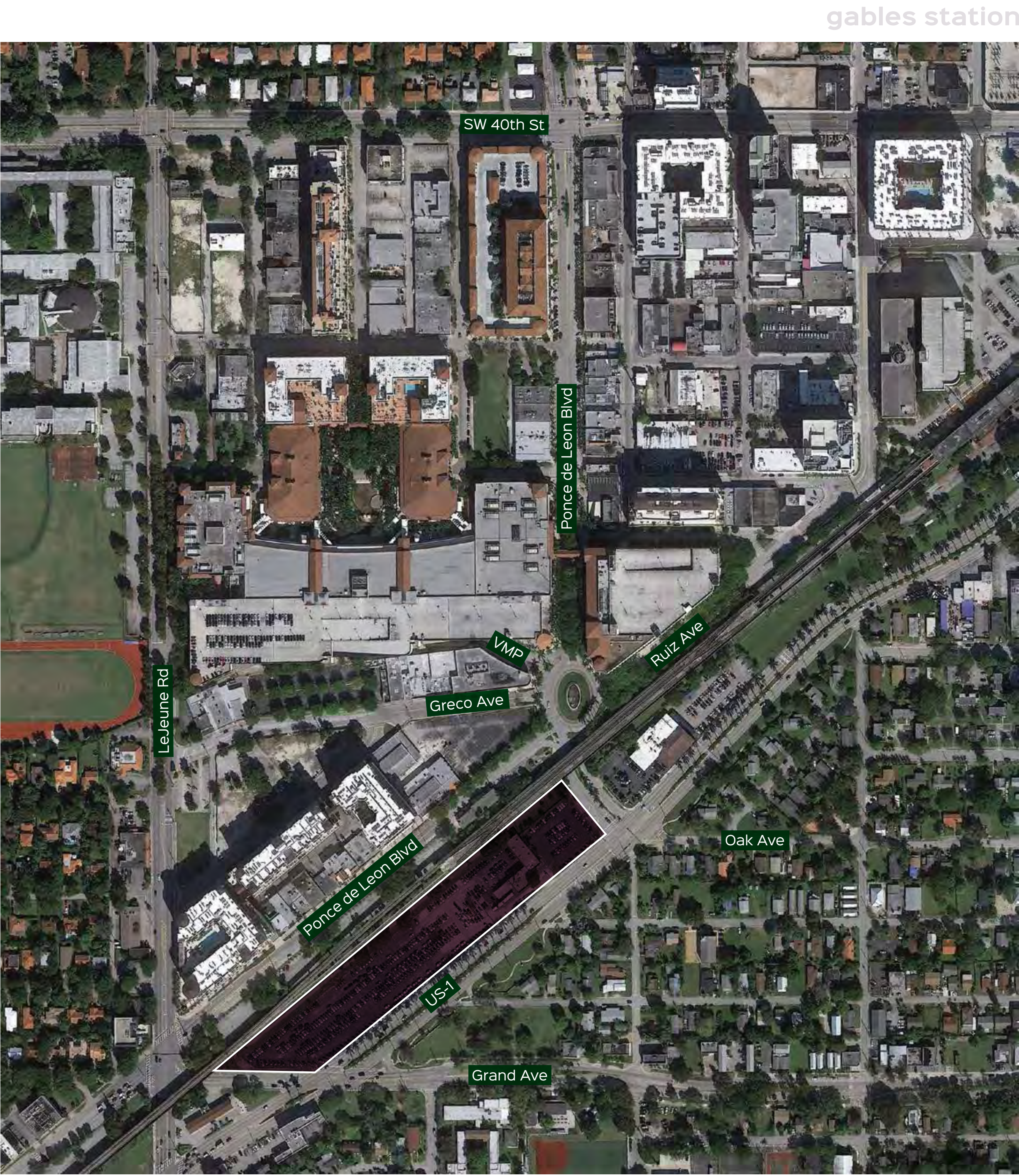
1.1 Project Background

The proposed Gables Station project will be located on the west side of US-1 between Ponce de Leon Boulevard and Grand Avenue in Coral Gables, Florida (See Exhibit 1). The site is located within the Gables Re-development Infill District (GRID), the city’s traffic concurrency exception area. It should be noted that the site was previously approved for a project proposing 295,000 GLSF of shopping center. The current proposed development program shows a substantial decrease of project trips generated when compared to the previously approved Gables Station Project. The current development program includes 526 apartment units and 75,294 SF of retail space. Access to and from the project site will be accommodated on East Ponce de Leon Boulevard, South Ponce de Leon Boulevard, US-1, and Grand Avenue. The project proposes an internal roadway that runs east/west along the site and connects all the project driveways. The onsite parking garage will access the internal roadway and provides 968 parking spaces. The site plan is provided in Appendix A.

This traffic study is consistent with the methodology previously discussed with and agreed to by the city of Coral Gables Public Works Department. For the purpose of this traffic study, project build-out is anticipated in 2018.

1.2 Study Objective

The purpose of the study is to provide a traffic study that meets the requirements of the city of Coral Gables for the project. This study includes vehicular flow, trip generation, and intersection analyses.



- VMP Village of Merrick Park
- Project Location

Exhibit 1
Location Map

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1.3 Study Area and Methodology

The analysis undertaken follows the study methodology previously discussed with and approved by the city of Coral Gables Public Works Department (See Appendix B). A synopsis of the methodology is as follows:

- Traffic Counts (Intersections) – Two-hour turning movement counts were collected for the AM (7-9 AM) and PM (4-6 PM) hours on January 20, 2016 at the following intersections:
 - LeJeune Road / Ponce de Leon Boulevard / Grand Avenue (S)
 - LeJeune Road / US-1 (S)
 - Grand Avenue / US-1 (S)
 - Ponce de Leon Boulevard / US-1 (S)
 - LeJeune Road / Ponce de Leon Boulevard / Greco Avenue / Ruiz Avenue (R)
 - LeJeune Road / Bird Road (S)
 - Ponce de Leon Boulevard / Bird Road (S)

S= Signalized

- Background Traffic. Average Annual Daily Traffic (AADT) volumes were reviewed to determine the appropriate background growth applicable to this area. This growth rate was applied to existing traffic counts to establish future traffic conditions without project for the anticipated project buildout year.
- Committed Developments. The city was consulted to determine any committed development in the vicinity of the project site. Traffic associated with these projects was considered in the analysis.
- Project Traffic. Trip generation for the project was estimated using trip generation information published by the Institute of Transportation Engineers (ITE) publication *Trip Generation Manual*, 9th Edition. Net new external project traffic was assigned to the adjacent street

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network using the appropriate cardinal distribution from the *Miami-Dade 2040 Long Range Transportation Plan*, published by the Metropolitan Planning Organization.

- Future Traffic Conditions. Project traffic was combined with projections of future traffic with and without project. Intersection capacity analyses were performed for this condition.
- Circulation Analysis/Plan – A circulation plan is provided depicting the project site, driveways, delivery areas, location of street signs/signals, crosswalks, sidewalks, location of bus facilities, bike facilities, adjacent streets configuration (travel lanes, etc.) including names, on-street parking and any other pertinent transportation features in the vicinity of this project.

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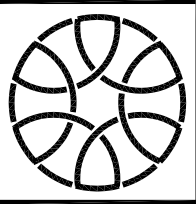
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EXISTING CONDITIONS

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2.0 EXISTING CONDITIONS

Data collection for this study included roadway characteristics, intersection traffic counts, signal timing, and seasonal adjustment factors. The data collection effort is described in the following sections.

2.1 Roadway Characteristics

US-1 / Dixie Highway (SR 5)

US-1 is a major arterial state roadway that provides northeast/southwest access throughout Miami-Dade County. Within the study area, US-1 is a two-way, six-lane, divided roadway. On-street parking is not permitted. The posted speed limit is 45 mph. The Florida Department of Transportation (FDOT) has jurisdiction over US-1.

Ponce de Leon Boulevard (Northeast / Southwest)

Within the study area, Ponce de Leon Boulevard is a minor arterial that runs northeast/southwest. Ponce de Leon Boulevard is a two-way, two-lane, undivided roadway. On-street parking is provided throughout most of the corridor. The posted speed limit is 35 mph. The City of Coral Gables has jurisdiction over Ponce de Leon Boulevard.

Ponce de Leon Boulevard (North / South)

Ponce de Leon Boulevard is a minor arterial that provides north/south access throughout the city of Coral Gables Central Business District (CBD). Within the study area, Ponce de Leon Boulevard is a two-way, four-lane, undivided roadway. On-street, metered, parking is provided on the east and west side of the roadway south of Bird Road. The posted speed limit is 30 mph. The city of Coral Gables operates and maintains Ponce de Leon Boulevard.

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SW 42nd Avenue (LeJeune Road)

SW 42nd Avenue is a major arterial that provides north/south access throughout Miami-Dade County. Within the study area, LeJeune Road is a two-way, four-lane, divided roadway. On street parking is prohibited. The speed limit is 40 mph. The Florida Department of Transportation (FDOT) has jurisdiction over LeJeune Road.

SW 40th Street (Bird Road)

SW 40th Street is a major arterial that provides east/west access throughout Miami-Dade County. Within the study area, SW 40th Street is a two-way, four-lane, divided roadway. An exclusive right turn lane is provided in the eastbound direction at every intersection within the study area. On street parking is prohibited. The speed limit is 40 mph. The Florida Department of Transportation (FDOT) has jurisdiction over Bird Road.

Grand Avenue

Grand Avenue is a county collector roadway that provides east/west access within the study area. West of US-1, Grand Avenue is a two-way, four-lane, undivided roadway and on-street parking is not permitted. East of US-1, Grand Avenue is a two-way, two-lane, divided roadway and on-street parking is permitted on this segment. The speed limit is not posted within the study limits. Miami-Dade County has jurisdiction over Grand Avenue.

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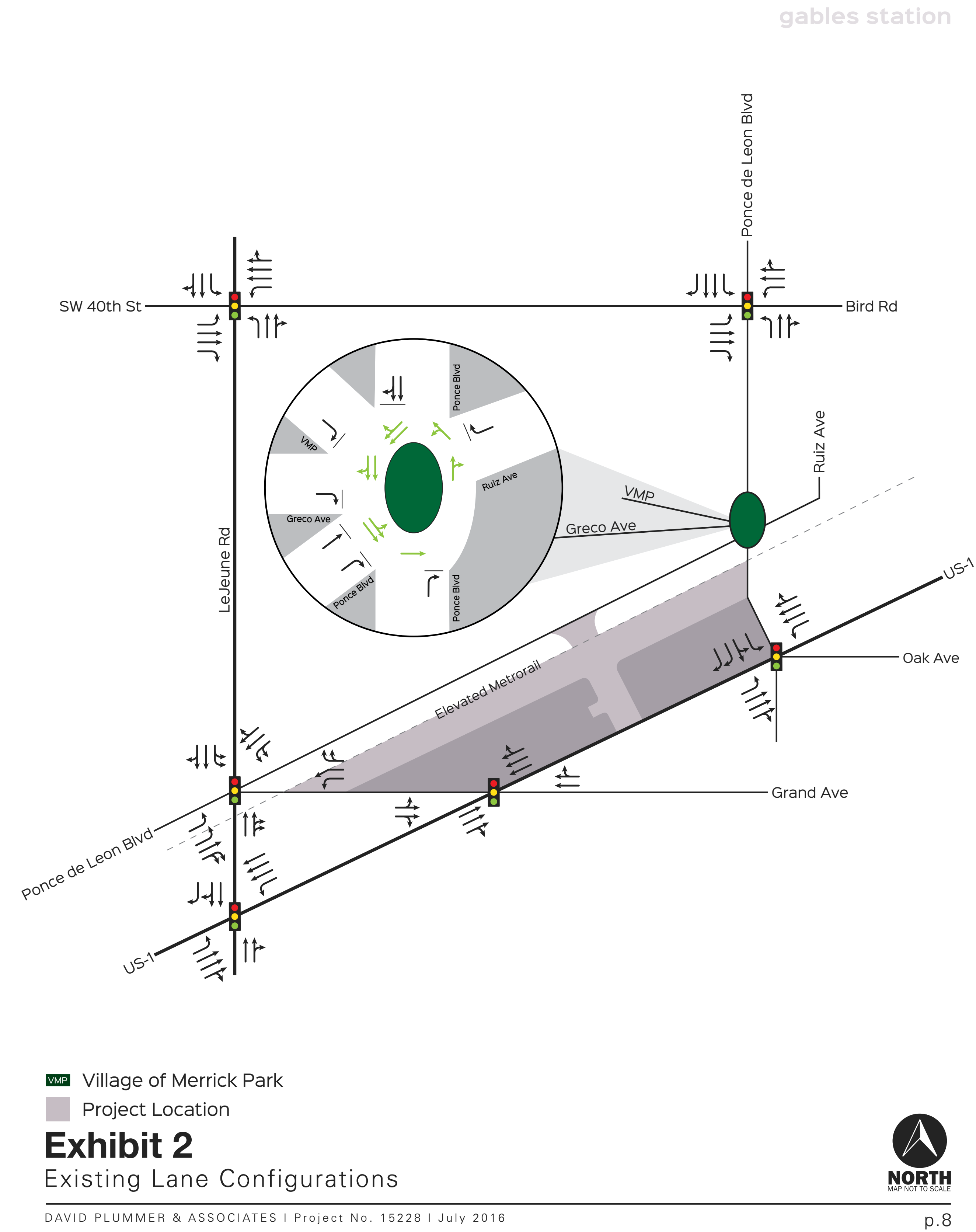
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Traffic Impact Study

2.2 Traffic Counts

Vehicle turning movement counts were taken on January 20, 2016 at the study intersections during the AM (7-9 AM) and PM (4-6 PM) peak periods. The counts were adjusted to reflect average annual daily traffic conditions using the latest weekly volume adjustment factors obtained from FDOT. A weekly volume adjustment factor of 1.02 (Miami-Dade County South) corresponding to the dates of the counts was used to adjust the raw turning movement counts to peak seasonal conditions. Traffic counts are provided in Appendix C.

2.3 Intersection Data

Signal timing data was obtained from Miami-Dade County for the signalized intersections analyzed in this study. This information was used for the signal phasing and timing required for the intersection capacity analysis. A field survey was also conducted to obtain the intersection lane configurations to be used in the intersection analysis. Exhibit 2 shows the existing lane configurations at the analyzed intersections. Existing volumes for the morning and afternoon peak hour at the intersections analyzed are shown in Exhibit 3. The signal timings are also provided in Appendix C.



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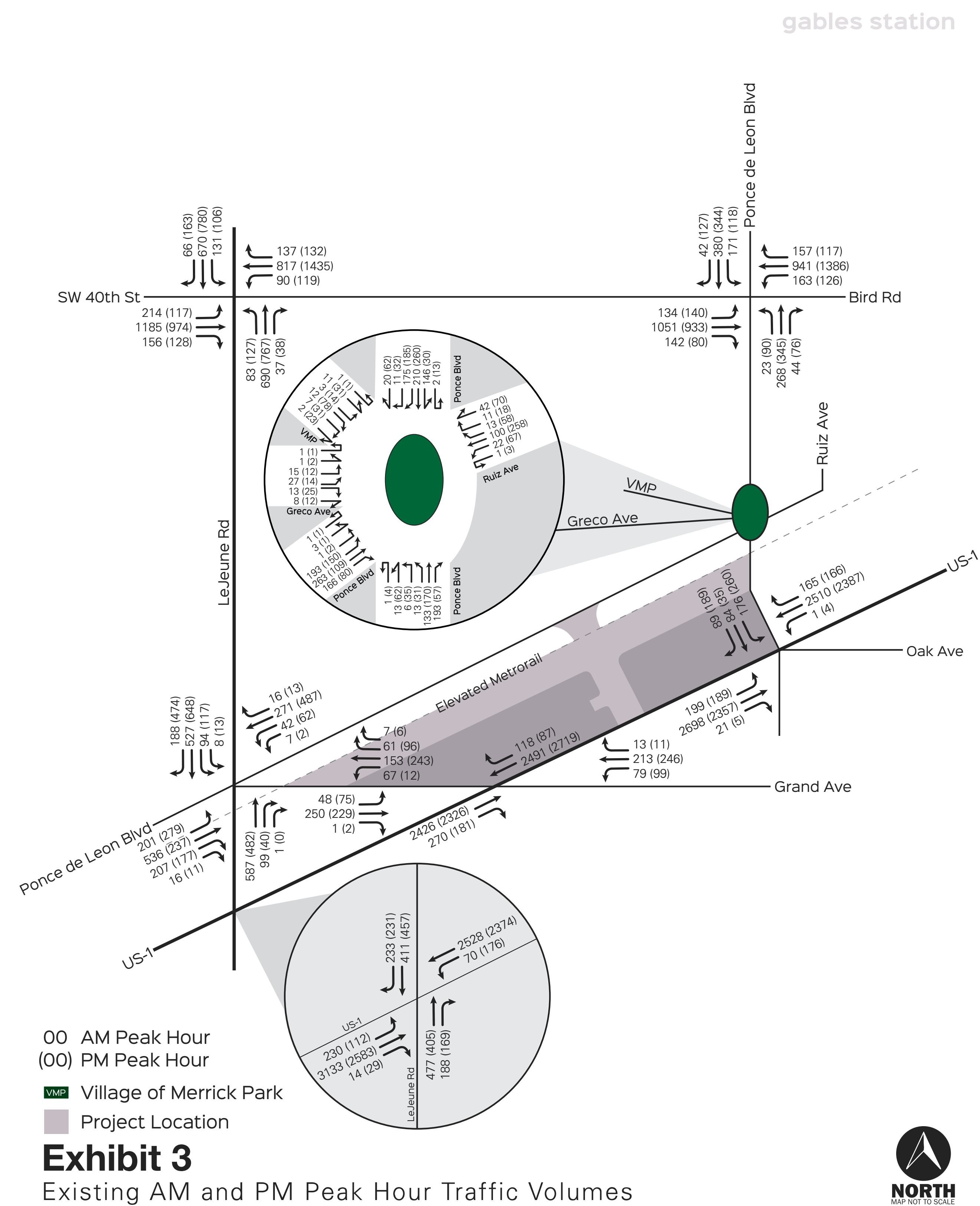
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2.4 Walking / Other Modes of Transportation

Pedestrian activity is an essential element within this area of Coral Gables. The Coral Gables Trolley service (which traverses the Ponce de Leon Boulevard corridor) provides frequent service to the project area. The Douglas Road Metrorail Station is located approximately 0.3 miles from the project site. The project area is also serviced by multiple Miami-Dade Transit bus routes. The Project site is located in an area where pedestrian activity is common between existing site and surrounding properties.

- Village of Merrick Park is located just north of Ponce de Leon Boulevard
- The University of Miami is approximately one mile south on Ponce de Leon Boulevard

See Section 5.0 of this report for the circulation plan including details on pedestrian access and available transit.

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EXISTING CONDITIONS

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Traffic Impact Study

2.5 Intersection Capacity Analysis

The proposed project is located within the city of Coral Gables Redevelopment and Infill District (GRID), which is a Transportation Concurrency Area established by the city to promote development within its boundaries. In essence, this ordinance establishes that roadways within the geographical area of the GRID are exempt from the citywide traffic LOS Standards.

The Synchro software was used to perform intersection capacity analysis at the analyzed intersections. Synchro is a macroscopic analysis and optimization software application that implements the Intersection Capacity Utilization method for determining intersection capacity. Exhibit 4 shows the resulting LOS for existing conditions during morning and afternoon peak periods. The results show that the overall LOS of all intersections is within the city’s LOS standards. Analysis worksheets are included in Appendix D.

Gables Station
Traffic Impact Study

Exhibit 4: Existing Intersection Capacity Analysis
Weekly AM and PM Peak Hour Conditions

Intersection	Signalized/ Un-signalized/ Roundabout	Direction	AM Peak LOS	PM Peak LOS	LOS Standard
LeJeune Road / Ponce de Leon Boulevard / Grand Avenue	Signalized	NB SB NEB SWB WB Overall	C D E E+25 E+6 E	C D E E+45 E+86 E	E + 20 E + 20 E + 50 E + 50 E + 20 E+
LeJeune Road / US-1	Signalized	NB SB NEB SWB Overall	E+79 E C D D	E+10 E E B D	E + 20 E + 20 E + 50 E + 50 E+
Grand Avenue / US-1	Signalized	NEB SWB EB WB Overall	B A E+15 E+4 B	B A E+45 E+8 B	E + 50 E + 50 E + 20 E + 20 E+
Ponce de León Boulevard / US-1	Signalized	SB NEB SWB Overall	E+39 A C C	E+60 C B C	E + 20 E + 50 E + 50 E+
LeJeune Road / Ponce de Leon Boulevard / Greco Avenue/ Ruiz Avenue	Roundabout	NB SB NEB SWB EB SEB Overall	C A B A A A B	B A A C A B B	E + 20 E + 20 E + 50 E + 50 E + 20 E + 20 E+
LeJeune Road / SW 40 th Street (Bird Road) /	Signalized	NB SB EB WB Overall	E E D D D	E E C C D	E + 20 E + 20 E + 20 E + 20 E + 20
Ponce de Leon Boulevard / SW 40 th Street (Bird Road) /	Signalized	NB SB EB WB Overall	E D D D D	E E C C D	E + 20 E + 20 E + 20 E + 20 E + 20

Source: David Plummer & Associates

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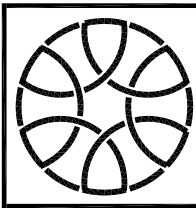
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PLANNED AND PROGRAMED ROADWAY IMPROVEMENTS

Gables Station
Traffic Impact Study

3.0 PLANNED AND PROGRAMED ROADWAY IMPROVEMENTS

The 2015 Miami-Dade County *Transportation Improvement Program (TIP)* and the *2040 Long Range Transportation Program* (LRTP) were reviewed to identify any programmed or planned projects within the limits of the study area established. Projects within the roadway segments under study are listed below.

SR 5/ US-1/ South Dixie Highway

- DT 4334851 Intersection improvements at Grand Avenue
 - o Minor signing and pavement marking restriping (T-8)
 - o Replace 3 section signal head for 5 section signal head (T-9)

Roadway improvement information is included in Appendix E.

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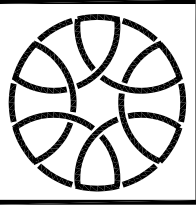
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FUTURE TRAFFIC CONDITIONS

Gables Station
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4.0 FUTURE TRAFFIC CONDITIONS

4.1 Background Traffic and Committed Developments

Average Daily Traffic (ADT) counts published by the Miami-Dade Public Works Department and the FDOT were reviewed to determine historic growth in the area. This analysis indicated that the traffic has fluctuated in the past years. A conservative 0.5% annual growth rate was used for this study. Historic growth documentation is included in Appendix C.

The city was consulted to determine any committed development in the vicinity of the project site. Four committed developments were considered for estimating future traffic volumes in this study: Merrick Manor, 4311 Ponce, the Collection Residences, and Gables Pointe Plaza. Exhibit 5 provides a tabulation of AM and PM peak hour trips generated by the committed development, along with the approved land uses. Committed development information is also included in Appendix E.

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Exhibit 5: Committed Development Trip Generation

Project	ITE Land Use	Size/Units	AM Peak Hour Vehicle Trips*			PM Peak Hour Vehicle Trips*		
			In	Out	Total	In	Out	Total
Merrick Manor	Apartments (Land Use 220)	188 DU	19	77	96	79	42	121
	Specialty Retail (Land Use 826)	1,900 SF	0	0	0	2	3	5
	Restaurant (Land Use 931)	5,600 SF	3	2	5	28	14	42
4311 Ponce	Office Building (Land Use 710)	24,133 SF	33	5	38	6	30	36
	Specialty Retail (Land Use 826)	11,457 SF	0	0	0	14	17	31
	Residential Condominiums (Land Use 230)	8 DU	1	6	7	5	3	8
The Collection Residences	Residential Condominiums (Land Use 230)	130 DU	11	53	64	50	25	75
	Automobile Sales (Land Use 841)	12,000 SF	17	6	23	13	18	31
	Supermarket (Land Use 850)	20,000 SF	42	26	68	97	93	190
Gables Pointe Plaza	High Turnover Sit Down Restaurant (Land Use 932)	275 Seats	66	63	129	63	50	113
	Office Building (Land Use 710)	2,5003 SF	3	0	3	1	3	4

* Gross vehicle trip ends. Appendix D reflects adjustments for existing land uses, pass-by, internal, and transit trips.

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FUTURE TRAFFIC CONDITIONS

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4.2 Future without Project Intersection Capacity Analysis

Future without project conditions was obtained by adding background traffic with committed development trips. Exhibit 6 shows the resulting LOS for morning and afternoon peak conditions for future without project. Exhibit 7 shows the projected turning movements for future without project traffic. All intersections continue to operate within the city’s LOS standard. Capacity worksheets are included in Appendix D.

Gables Station
Traffic Impact Study

Exhibit 6: Future without Project Intersection Capacity Analysis
Weekly AM and PM Period Conditions

Intersection	Signalized/ Un-signalized/ Roundabout	Direction	AM Peak LOS	PM Peak LOS	LOS Standard
LeJeune Road / Ponce de Leon Boulevard / Grand Avenue	Signalized	NB SB NEB SWB WB Overall	C D E E+31 E+6 E	C D E E+54 E+85 E	E + 20 E + 20 E + 50 E + 50 E + 20 E+
LeJeune Road / US-1	Signalized	NB SB NEB SWB Overall	E+84 E C D D	E+14 E E B E	E + 20 E + 20 E + 50 E + 50 E+
Grand Avenue / US-1	Signalized	NEB SWB EB WB Overall	B A E+40 E+12 B	B A E+81 E+18 C	E + 50 E + 50 E + 20 E + 20 E+
Ponce de León Boulevard / US-1	Signalized	SB NEB SWB Overall	E+45 A C C	E+63 D B C	E + 20 E + 50 E + 50 E+
LeJeune Road / Ponce de Leon Boulevard / Greco Avenue/ Ruiz Avenue	Roundabout	NB SB NEB SWB EB SEB Overall	C A B A A A B	B B A D A B B	E + 20 E + 20 E + 50 E + 50 E + 20 E + 20 E+
LeJeune Road / SW 40 th Street (Bird Road) /	Signalized	NB SB EB WB Overall	E E D D D	E E D D D	E + 20 E + 20 E + 20 E + 20 E + 20
Ponce de Leon Boulevard / SW 40 th Street (Bird Road) /	Signalized	NB SB EB WB Overall	E D D D D	E E C C D	E + 20 E + 20 E + 20 E + 20 E + 20

Source: David Plummer & Associates

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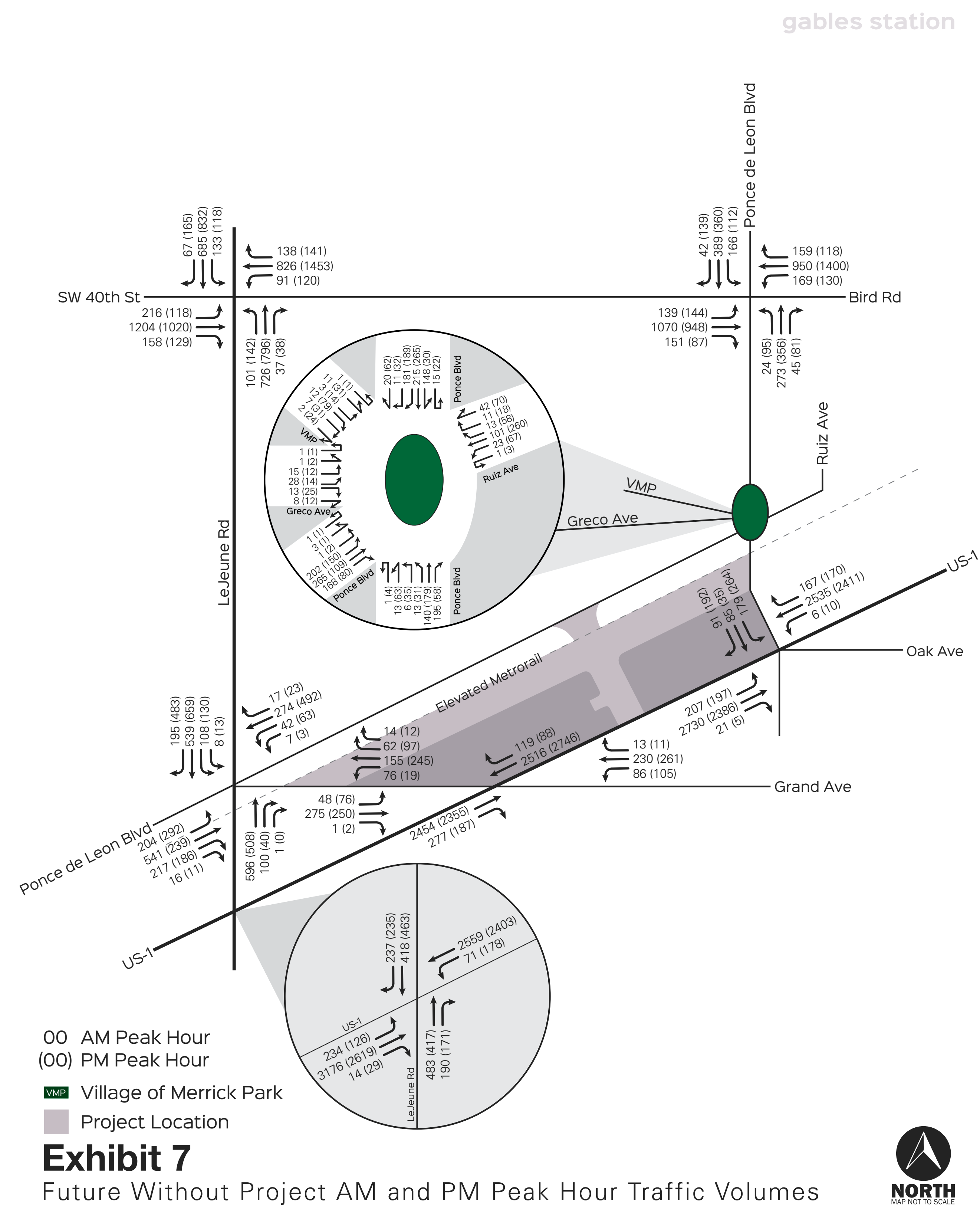
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FUTURE TRAFFIC CONDITIONS



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Traffic Impact Study

4.3 Project Trip Generation

Trip generation for the proposed project and the existing use was estimated using the Institute of Transportation Engineers (ITE) Trip Generation Manual, 9th Edition. This manual provides gross trip generation rates and/or equations by land use type. These rates and equations estimate vehicle trip ends at a free-standing site’s driveways. See Appendix F for project trip generation worksheets.

The proposed development plan incorporates residential and retail land uses, which can satisfy the work trip, dining, and retail needs for some residents, employees, and visitors without making a trip off-site. An internalization matrix was developed to establish the appropriate number of internal project trip capture. Internal capture rates used are also included in Appendix F.

The Trip Generation Handbook User’s Guide and Handbook includes data on pass-by trips. Pass-by trips are those trips that are attracted from the traffic passing the site on an adjacent street. Since the pass-by trips are already on the street system, the total trip generation from the site was adjusted to estimate the new external traffic actually added to the street system. The average pass-by rate published by ITE for Shopping Center was used to establish the pass-by component. Although ITE only provides data for the PM peak hour, the retail component will attract patrons from the adjacent traffic flow throughout the day. Therefore, the ratio of the AM to PM peak hour trips for this use was used to calculate AM pass-by percentages.

The proposed project is located within the City’s Central Business District (CBD). Therefore, pedestrian activity is expected between the site and adjacent residential areas as well as other destinations (retail, restaurants and services). The project site is also in an area served by bus routes from Miami-Dade Transit, the Coral Gables trolley and is just south of the Douglas Road Metrorail Station. Therefore, a conservative 10% adjustment was applied to the trip generation to account for transit trips and a conservative 10% for pedestrian trips (taken from apartment only). The project trip generation summary is provided in Exhibit 8.

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FUTURE TRAFFIC CONDITIONS

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Exhibit 8: Project Trip Generation Summary							
Proposed ITE Land Use Designation ¹	Size/Units	AM Peak Hour Vehicle Trips			PM Peak Hour Vehicle Trips		
		In	Out	Total	In	Out	Total
Apartments (Land Use 220)	526 DU	52	209	261	200	107	307
Shopping Center (Land Use 820)	75,294 SF	81	50	131	238	257	495
Subtotal Gross Trips		133	259	392	438	364	802
Pass-by Trip ² (Shopping center only)	11% (AM) 42% (PM)	-9	-6	-15	-101	-109	-210
Transit Trips	10%	-12	-25	-37	-34	-26	-60
Pedestrian Trips (Apartment only)	10%	-5	-21	-26	-20	-11	-31
Internal Capture ²	1.8% (AM) 23.9% (PM)	-3	-3	-6	-85	-85	-170
Net External Trips (Proposed)		104	204	308	198	133	331

¹ Based on ITE Trip Generation Manual, Ninth Edition,
² Based on ITE Trip Generation Manual User's Guide and Handbook, Ninth Edition

As mentioned earlier, this site was previously approved for a project proposing 295,000 GLSF of shopping center. The current proposed development program shows a substantial decrease of project trips when compared to the previously approved Gables Station Project trip generation (based on the Traffic Study Dec 2011). The approved project created a total of 780 net trips and the currently proposed development program results in 331 net trips; a 57% decrease in trips during the PM peak hour. The previously approved project trip generation summary is provided in Exhibit 8A as reference.

Exhibit 8A: Project Trip Generation Summary from Gables Station Traffic Study dated Dec 2011				
Proposed ITE Land Use Designation ¹	Size/Units	PM Peak Hour		
		In	Out	Total
Retail (Land Use 820)	294,429 GLSF	643	669	1312
Pass-By Trips ²	34%	-219	-227	-446
Transit/Pedestrian Trips	10%	-42	-44	-86
Net New Trips		382	398	780

¹ Based on ITE Trip Generation manual, 8th Edition. Source: David Plummer & Associates
² Based on average Pass-by trip percentage for Shopping Centers - ITE Trip Generation Handbook Table 5.4.

Gables Station
Traffic Impact Study

4.4 Project Trip Assignment

Project traffic was distributed and assigned to the study area using the Cardinal Distribution for TAZ 1096 shown in Exhibit 9. The Cardinal Distribution gives a generalized distribution of trips from a TAZ to other parts of Miami-Dade County. The distribution can be summarized as follows: 41.09% to the north, 12.88% to the south, 13.98% to the east, and 32.19% to the west. For estimating trip distribution for the project traffic, consideration was given to conditions such as the roadway network accessed by the project traffic, roadways available to travel in the desired direction, and attractiveness of traveling on a specific roadway. Project trip distribution for the proposed project is shown in Exhibit 10.

Exhibit 9: Cardinal Distribution
(TAZ 1096)

Direction	Distribution
NNE	21.33%
ENE	13.03%
ESE	0.95%
SSE	1.34%
SSW	11.54%
WSW	21.54%
WNW	10.63%
NNW	19.77%
Total	100.00%

Source: Miami-Dade Long Range Transportation Plan

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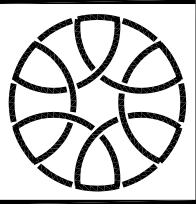
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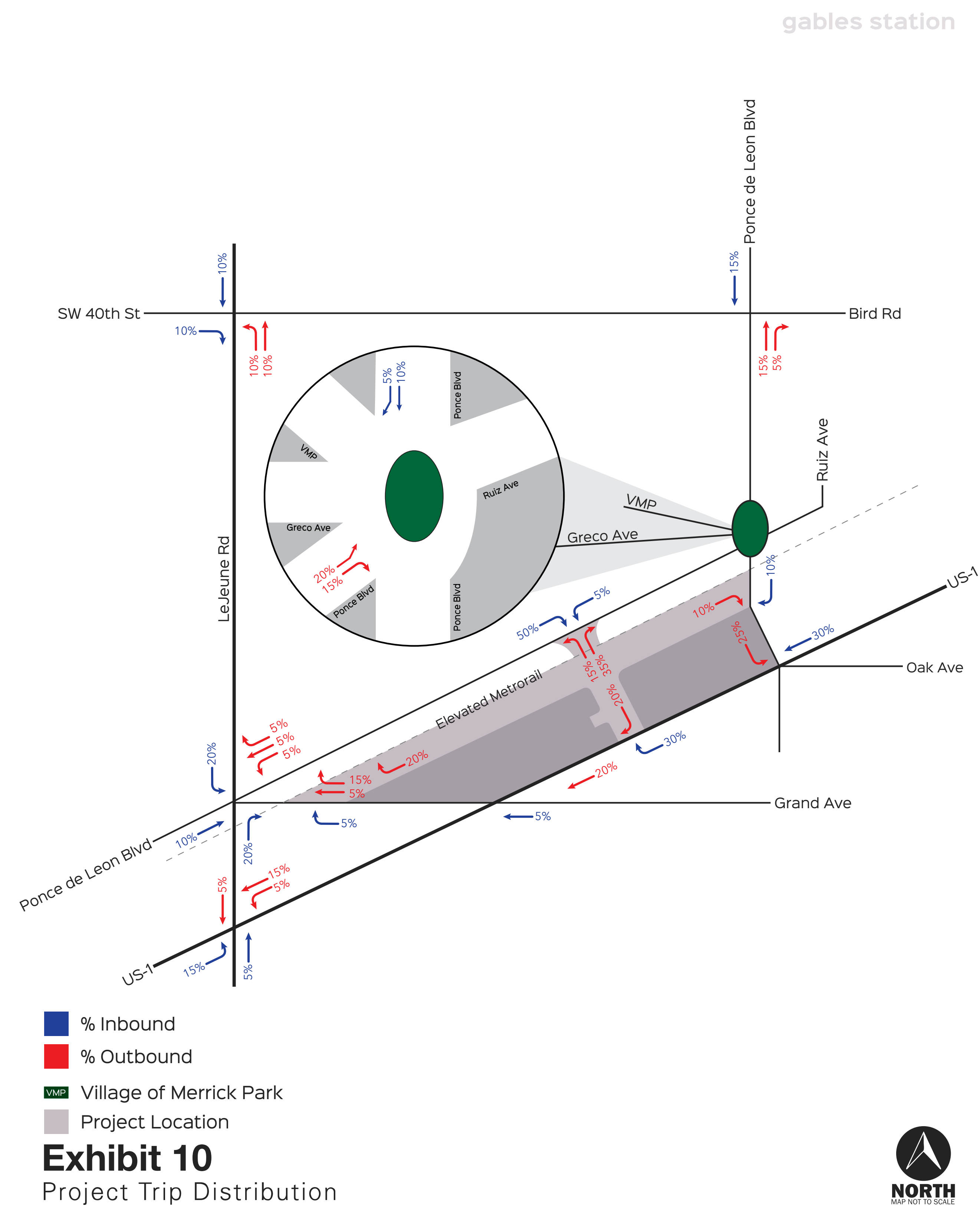
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FUTURE TRAFFIC CONDITIONS



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Traffic Impact Study

4.5 Future with Project Intersection Capacity Analysis

The trip assignments in the previous section, traffic projections for the project, committed developments and background growth were combined to obtain future traffic with project at the analyzed intersections. Exhibit 11 shows the resulting LOS for the morning and afternoon peak conditions for future with project. Capacity worksheets are included in Appendix D. Exhibit 12 shows the projected turning movement volumes for future with project. The analysis shows that all intersections analyzed are projected to operate within the city’s LOS standard during the morning and afternoon peak periods.

Although the analyzed intersections’ overall LOS is within the City’s LOS standards, minor signal timings adjustment are recommended during the AM and PM peak period to improve the operations of certain approaches. The minor signal timing adjustments are recommended below:

- Ponce de Leon Boulevard / LeJeune Road / Grand Avenue – adjust signal timing to provide more green time to the southwest and westbound movements
- LeJeune Road / US-1 – adjust signal timing to provide more green time to the northbound movements
- Grand Avenue / US-1 – adjust signal timing to provide more green time to the eastbound / westbound movements
- Ponce de Leon Boulevard / US-1– adjust signal timing to provide more green time to the southbound movements

Additionally, driveway analyses were performed for the project driveways at E Ponce de Leon Boulevard, S Ponce de Leon Boulevard, US-1, and Grand Avenue. The analysis shows that vehicles existing the US-1 driveway will experience some delay. It should be noted that, for unsignalized intersections, the software tends to overestimate delay measurements for the side streets (minor approach). Should the delays ever reach such a point shown by the software, motorists tend to use an alternate route. Intersection analysis results show that all other driveways are projected to operate at an acceptable LOS. The capacity worksheets for the project driveways are also included in Appendix D.

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Exhibit 11: Future with Project Intersection Capacity Analysis Weekday AM and PM Peak Period Conditions					
Intersection	Signalized/ Un-signalized/ Roundabout	Direction	AM Peak LOS	PM Peak LOS	LOS Standard
LeJeune Road / Ponce de Leon Boulevard / Grand Avenue ⁽¹⁾	Signalized	NB	D	C	E + 20
		SB	D	D	E + 20
		NEB	E	E	E + 50
		SWB	E+49	E+47	E + 50
		WB	E+7	E+27	E + 20
		Overall	E	E	E+
LeJeune Road / US-1 ⁽¹⁾	Signalized	NB	E+48	E+11	E + 20
		SB	E	E	E + 20
		NEB	C	D	E + 50
		SWB	E	B	E + 50
		Overall	D	D	E+
Grand Avenue / US-1 ⁽¹⁾	Signalized	NEB	B	B	E + 50
		SWB	A	A	E + 50
		EB	E+6	E+12	E + 20
		WB	E	E	E + 20
		Overall	B	B	E+
Ponce de León Boulevard / US-1 ⁽¹⁾	Signalized	SB	E+15	E+14	E + 20
		NEB	B	D	E + 50
		SWB	C	B	E + 50
		Overall	C	C	E+
LeJeune Road / Ponce de Leon Boulevard / Greco Avenue/ Ruiz Avenue	Roundabout	NB	D	B	E + 20
		SB	A	B	E + 20
		NEB	B	A	E + 50
		SWB	A	D	E + 50
		EB	A	A	E + 20
LeJeune Road / SW 40 th Street (Bird Road) /	Signalized	SEB	A	B	E + 20
		Overall	B	C	E+
		NB	E	E	E + 20
		SB	E	E	E + 20
		EB	D	D	E + 20
Ponce de Leon Boulevard / SW 40 th Street (Bird Road) /	Signalized	WB	D	D	E + 20
		Overall	D	D	E + 20
		NB	E	E	E + 20
		SB	D	E	E + 20
		EB	D	C	E + 20
		WB	D	C	E + 20
		Overall	D	D	E + 20

(1) AM and PM Peak LOS with Signal Timing Improvements

Gables Station
Traffic Impact Study

Exhibit 11 - Continued Project Driveway Intersection Capacity Analysis Weekday AM and PM Peak Period Conditions				
Intersection	Signalized/ Un-signalized/ Roundabout	Direction	AM Peak LOS	PM Peak LOS
Project Driveway / E Ponce de Leon Boulevard	Un-signalized	NWB	C	B
Project Driveway / S Ponce de Leon Boulevard	Un-signalized	NEB	B	B
Project Driveway / US-1	Un-signalized	SEB	F	F
Project Driveway / Grand Avenue	Un-signalized	SWB	A	A

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES,
FL 33134

PROPERTY OWNER

NP INTERNATIONAL
2903 Salzedo Street, Coral Gables, FL 33134
952 767 7500
www.np-international.com

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WWW.KIMLEY-HORN.COM CA 00000696

TRAFFIC CONSULTANT

DAVID PLUMMER & ASSOCIATES
TRAFFIC ENGINEERING • CIVIL ENGINEERING • TRANSPORTATION PLANNING

DATE	DESCRIPTION
07.14.2016	City Commission Submittal

INFO

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NOTE

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PROJECT NAME

GABLES STATION

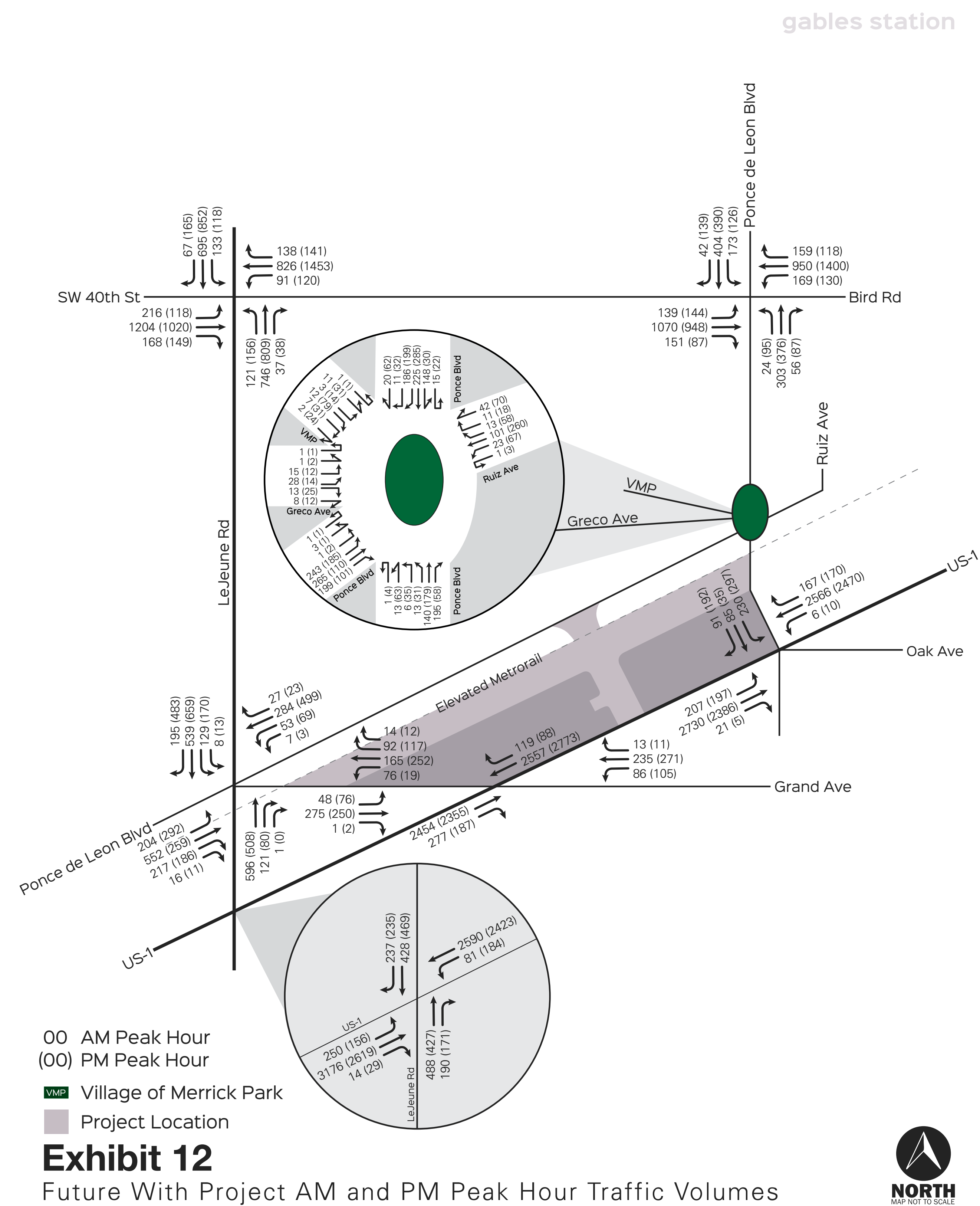
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FUTURE TRAFFIC CONDITIONS



JOB NAME AND LOCATION

GABLES STATION

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Gables Station
Traffic Impact Study

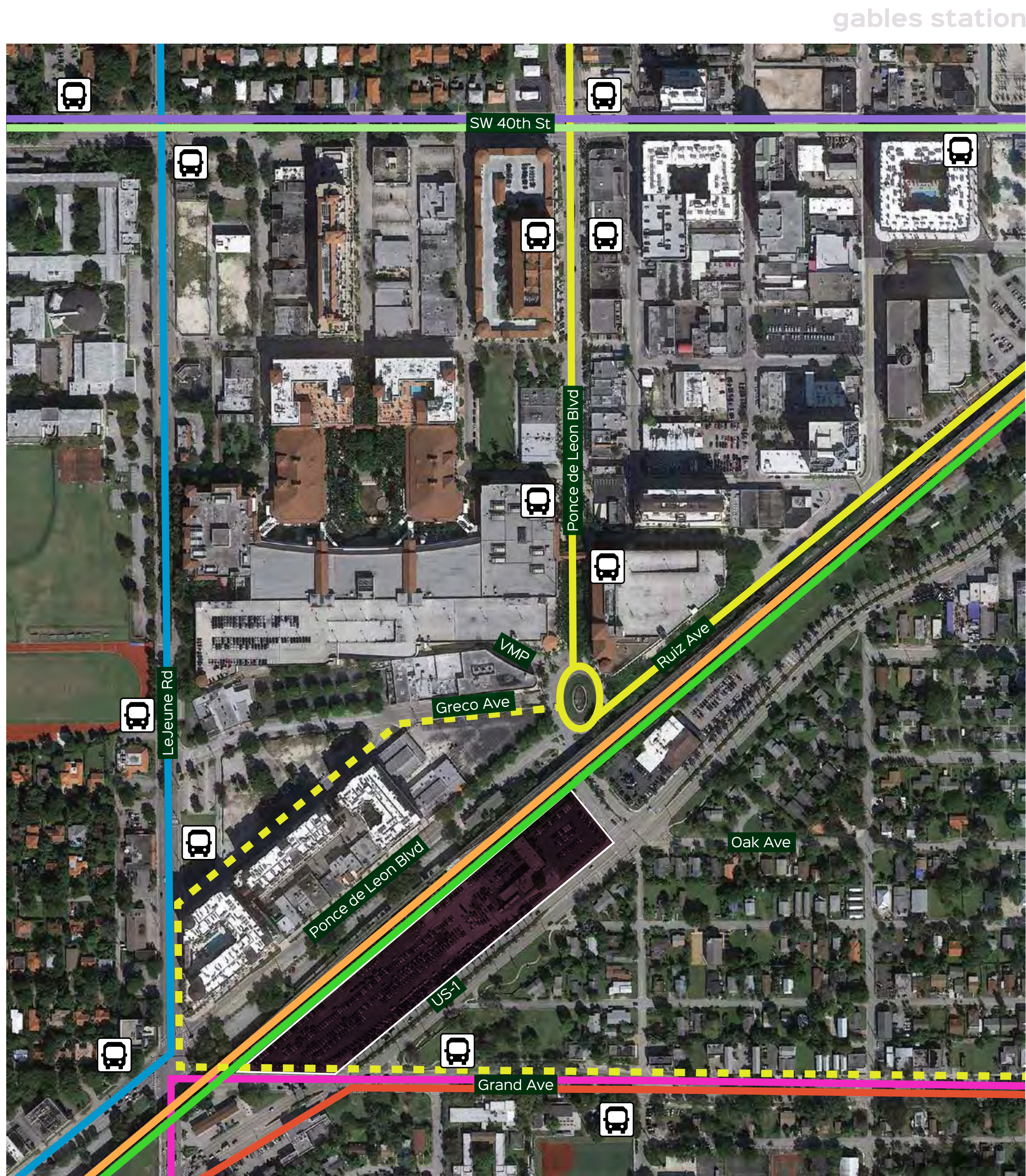
5.0 CIRCULATION PLAN

As mentioned before, access to and from the project site will be accommodated on four project driveways. The driveway accessing East Ponce de Leon Boulevard will allow all movements. The driveways accessing South Ponce de Leon Boulevard, US-1, and Grand Avenue will be right-in / right-out only. The project will propose three separated delivery truck load/off-load areas all of which will access the internal roadway of the project.

The area surrounding the project is served by transit. There are five bus routes (Routes: 40, 42, 48, 56 and 136) that traverse this area of Coral Gables. The closest bus stops to the project site are located on Ponce de Leon Boulevard west of Le Jeune Road. This project is located approximately 0.3 miles south of the Metrorail Douglas Station. The Coral Gables Trolley route also provides service to the project area. Exhibit 13 shows the available bus routes and bus stops in the area. Appendix F shows the bus route maps and schedules.

The project is located in an area that is conducive for pedestrian activities. Ponce de Leon Boulevard, Le Jeune Road, US-1 and Grand Avenue provide sidewalks on both sides of the road. Signalized intersections adjacent to the site have clearly marked crosswalks and provide pedestrian signals. No bike lanes are provided on adjacent roadways. However a bike path is provided under the elevated Metrorail (M-Path). The location of the project allows this pedestrian/bike path to cross directly adjacent to the site. An exhibit showing the proposed bike path/ vehicle crossing details is included in Appendix G. A circulation and mobility plan was prepared for the site (see Exhibit 14). The plan shows the project driveways, location of street signals, delivery areas, sidewalk connections, and pedestrian crosswalks.

CIRCULATION PLAN



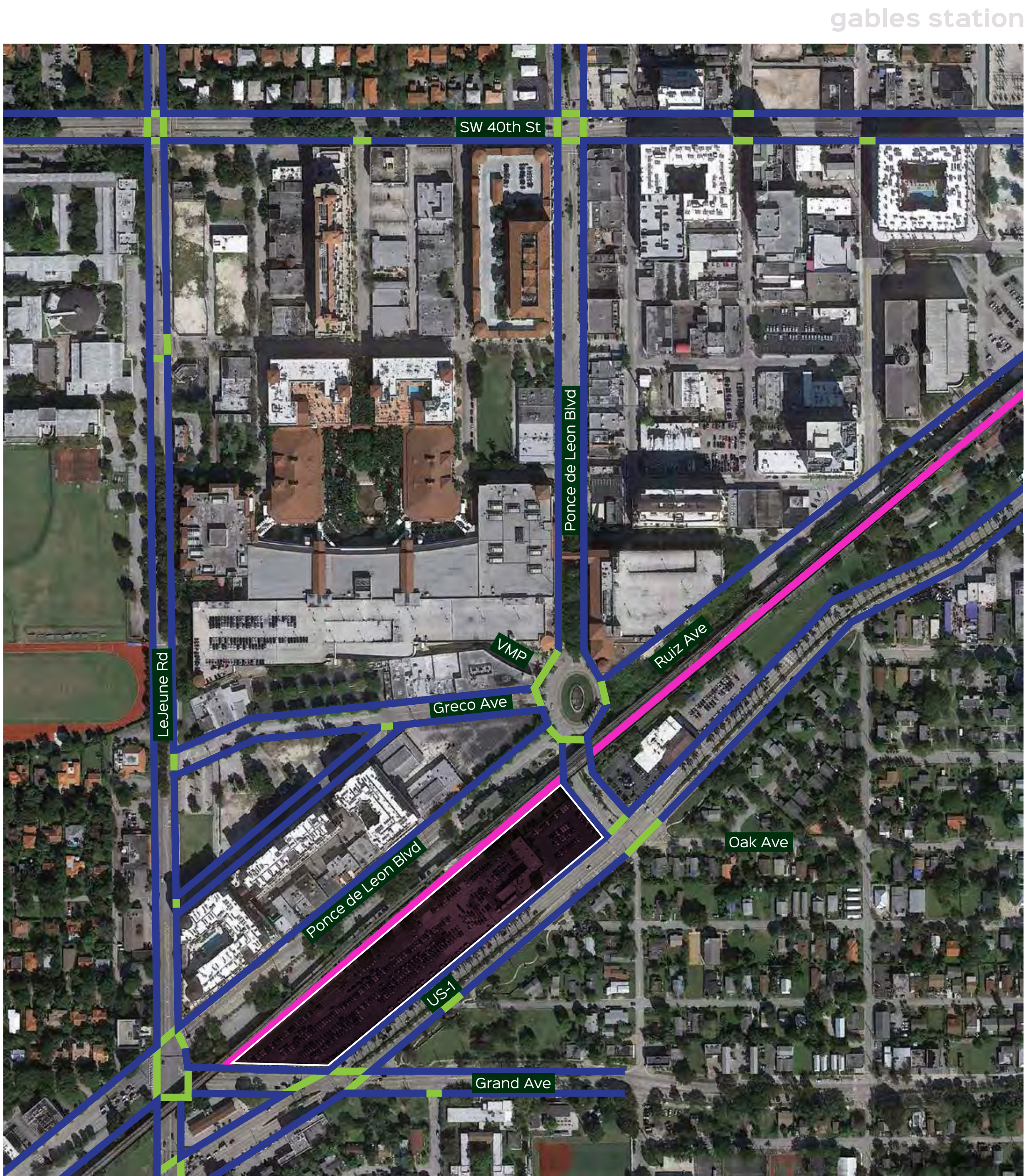
VMP Village of Merrick Park
Project Location

Exhibit 13
Circulation Plan - Bus Routes

Bus Route Legend			
Route 40	Route 42	Route 48	Route 56
Trolley	Trolley Extension	Metro	Metro
Bus Stop			



p.28



VMP Village of Merrick Park
Project Location

Exhibit 14
Circulation Plan - Mobility

Legend		
Sidewalk	Crosswalk	Path along Metro



p.29

JOB NAME AND LOCATION

GABLES STATION

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6.0 CONCLUSIONS

In addition, a mobility and circulation plan was completed as part of the study. The plan shows that the project area is currently served by various Miami-Dade Transit bus routes, the Metrorail, and the city of Coral Gables Trolley. The project is located in an area that is conducive for pedestrian and bicycle activities providing bike paths, ample sidewalks, and crosswalks.

As mention above, this site was previously approved for a project proposing a 295,000 GLSF shopping center. The currently proposed development program shows a substantial decrease of project trips when compared to the previously approved Gables Station Project. The approved project created a total of 780 net trips and the currently proposed development program results in 331 net trips; a 57% decrease in trips during the PM peak hour.

Page 30

PAGE NUMBER

SECTION 09

ADDITIONAL DOCUMENTS

LIST OF REPRESENTATIVES

CONTACT INFORMATION

Applicant and Developer

NP International, USA
2903 Salzedo Street
Coral Gables, FL 33134

Contact: Brent Reynolds
Ph: (952) 767-7500
Email: breynolds@np-international.com

Agent and Attorney

Mario Garcia-Serra
Gunster
Brickell World Plaza
600 Brickell Avenue
Suite 3500
Miami, FL 33131

Ph: (305) 376-6061
Email: MGarcia-Serra@gunster.com

Property Owner:

Gables Station LLC
2665 South Bayshore Drive, Suite 1200
Coconut Grove, FL 33133

Contact: Jeffery Berkowitz
Ph: (305) 854-2800
Email: JBerkowitz@berkowitzdev.com

Project Architect:

Gensler
801 Brickell Avenue
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Miami, FL 33131

Contact: Walter Trujillo
Ph: (305) 350-7063
Email: walter_trujillo@gensler.com

Project Traffic Consultant

Timothy J. Plummer, PE
David Plummer & Associates
1750 Ponce de Leon Boulevard
Coral Gables, Florida 33134

Ph: (305) 447-0900
Email: tim.plummer@dplummer.com

Project Civil Engineer

Julio Collier
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1221 Brickell Avenue, Suite 400
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Ph: (954) 535-5118
Email: julio.collier@kimley-horn.com

Landscape Architect

Robert A. Parsley A.S.L.A.
Geomantic Designs, Inc.
6800 SW 81st Street
Miami, FL 33143

Ph: (305) 665-9688 ext. 1
Email: rparsley@geomanticdesigns.com

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JOB NAME AND LOCATION

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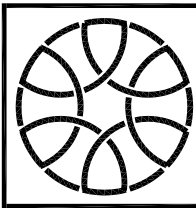
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LOBBYIST ISSUE APPLICATIONS



CITY OF CORAL GABLES
LOBBYIST
ISSUE APPLICATION

REGISTRATION #: _____

HAVE YOU BEEN RETAINED TO LOBBY ANY OF THE FOLLOWING FOR THE STATED PURPOSE?

CITY OFFICIALS: Mayor, City Commissioners, City Attorney, City Manager, City Clerk, Assistant City Manager, Special Assistant to City Manager, Heads or Directors of Departments, and their Assistant or Deputy, Police Major or Chief, Fire Major or Chief, Building and Zoning Inspectors, Board, Committee Members, or any City Official or staff.

FOR THIS PURPOSE: To encourage the passage, defeat or modification of any ordinance, resolution, action or decision of the City Commission; or any action, decision or recommendation of any Board, Committee or City Official.

IF THE FOREGOING APPLIES TO YOU, YOU ARE REQUIRED TO REGISTER AS A LOBBYIST AND TO FILE THE FOLLOWING INFORMATION, UNDER OATH, WITH THE CITY CLERK FOR EACH ISSUE ADDRESSED. ISSUE FEE: NO CHARGE, PROVIDING YOU HAVE A CURRENT ANNUAL LOBBYIST REGISTRATION DOCUMENT ON FILE.

Print Your Name Mario Garcia-Serra
LOBBYIST

Print Your Business Name Gunster, Yoakley & Stewart, P.A.

Business Telephone Number (305) 376-6000

Business Address 600 Brickell Avenue, Suite 3500, Miami, Florida 33131
ADDRESS CITY, STATE ZIP CODE

Corporation, Partnership, or Trust Represented:

Principal Name: NP International USA, LLC

Principal Address: 2903 Salzefo Street, Coral Gables, FL 33134 Telephone Number: 952-767-7500

ISSUE: Describe in detail, including address, if applicable, of the specific issue on which you will lobby: (Separate Application is required for each specific issue)

Obtaining the necessary land use and zoning approvals for proposed development of the site

located at 251 South Dixie Highway in the City of Coral Gables as a mixed use project along with,

if necessary, the acquisition of an abutting strip of property owned by the City of Coral Gables.

Lobbyist Form – Issue Application - Revised 6/30/10) 1 of 2

I Mario Garcia-Serra hereby swear or affirm under penalty of perjury that all the facts contained in this Application are true and that I am aware that these requirements are in compliance with the provisions of the City of Coral Gables Ordinance No. 2006-11, governing Lobbying.

Mario Garcia-Serra Signature of Lobbyist 12/7/15 Date

STATE OF FLORIDA }
COUNTY OF DADE }

BEFORE ME personally appeared Mario Garcia-Serra to me well known and known to me to be the person described in and who executed the foregoing instrument, and acknowledged to and before me that he/she executed said instrument for the purposes therein expressed.

WITNESS my Hand and Official Seal this _____

XX Personally Known
Produced ID



For Office Use Only

Data Entry Date: _____, 20____ Entered By: _____

Annual Fees Waived for Not-for-Profit Organization. Please attach documentary proof.

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES, FL 33134

PROPERTY OWNER

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952 767 7500
www.np-international.com

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Miami, FL 33131

305-350-7070

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JORGE L. HERNANDEZ ARCHITECT

FLORIDA REGISTRATION # 9843
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305.774.0022

CIVIL ENGINEERS

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TRAFFIC CONSULTANT

DAVID PLUMMER & ASSOCIATES
TRAFFIC ENGINEERING • CIVIL ENGINEERING • TRANSPORTATION PLANNING

DATE	DESCRIPTION
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LOBBYIST ISSUE APPLICATIONS



CITY OF CORAL GABLES
LOBBYIST ANNUAL REGISTRATION APPLICATION
FOR EACH PRINCIPAL REPRESENTED

REGISTRATION #: _____

HAVE YOU BEEN RETAINED TO LOBBY ANY OF THE FOLLOWING FOR THE STATED PURPOSE?

CITY OFFICIALS: Mayor, City Commissioners, City Attorney, City Manager, City Clerk, Assistant City Manager, Special Assistant to City Manager, Heads or Directors of Departments, and their Assistant or Deputy, Police Major or Chief, Fire Major or Chief, Building and Zoning Inspectors Board, Committee Members, or any other City Official or staff.

FOR THIS PURPOSE: To encourage the approval, disapproval, adoption, repeal, passage, defeat or modification of any ordinance, resolution, action or decision of the City Commission; or any action, decision or recommendation of the City Commission, any Board, Committee or City Official.

IF THE FOREGOING APPLIES TO YOU, YOU ARE REQUIRED TO REGISTER AS A LOBBYIST

Print Your Name Mario Garcia-Serra LOBBYIST

Print Your Business Name, if applicable Gunster, Yoakley & Stewart, P.A.

Business Telephone Number 305-376-6000

Business Address 600 Brickell Avenue, Suite 3500, Miami, Florida 33131
ADDRESS CITY, STATE ZIP CODE

Federal ID#: 59-1450702

State the extent of any business or professional relationship you have with any current member of the City Commission. None

PRINCIPAL REPRESENTED:
NAME NP International USA, LLC COMPANY NAME, , IF APPLICABLE
BUSINESS ADDRESS 2903 Salzedo Street TELEPHONE NO.: 952-767-7500
Coral Gables, FL 33134

Lobbyist – Annual Registration Application (Revised 06/30/10)

Page 1 of 2

ANNUAL REPORT: On October 1st of each year, you are required to submit to the City Clerk a signed statement under oath listing all lobbying expenditures in excess of \$25.00 for the preceding calendar year. A statement is required to be filed even if there were no expenditures.

LOBBYIST ISSUE APPLICATION: Prior to lobbying for a specific issue, you are required to fill out a Lobbyist Issue Application form with the Office of the City Clerk; stating under oath, your name, business address, the name of each principal who employed you to lobby, and the specific issue on which you wish to lobby.

NOTICE OF WITHDRAWAL: If you discontinue representing a particular client, a notice of withdrawal is required to be filed with the City Clerk.

ANNUAL LOBBYIST REGISTRATION FEE: This Registration must be on file in the Office of the City Clerk prior to The filing of an Issue Application to lobby on a specific issue, and payment of a \$150.00 Lobbyist Registration Fee is required.

I Mario Garcia-Serra hereby swear or affirm under penalty of per-
jury that I have read the provisions of the City of Coral Gables Ordinance 2006-
11, governing Lobbying and that all of the facts contained in this Registration
Application are true and that I agree to pay the \$150.00 Annual Lobbyist Regis-
tration Fee.

Mario Garcia-Serra
Signature of Lobbyist

STATE OF FLORIDA)
COUNTY OF DADE)

BEFORE ME personally appeared Mario Garcia-Serra to me well known and known to me to be the person described in and who executed the foregoing instrument, and acknowledged to and before me that he/she executed said instrument for the purposes therein expressed.

WITNESS my Hand and Official Seal this _____
XX Personally Known
Produced ID
Notary Public
State of Florida
\$150.00 Fee Paid _____ Received By _____ Date: _____
Fee Waived for Not-for-Profit Organizations (documentary proof attached.) _____

For Office Use Only

Data Entry Date: _____, 20____ Entered By: _____

JOB NAME AND LOCATION

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DAVID PLUMMER & ASSOCIATES
TRAFFIC ENGINEERING • CIVIL ENGINEERING • TRANSPORTATION PLANNING

DATE	DESCRIPTION
07.14.2016	City Commission Submittal

APPLICABLE ZONING RESOLUTION

AN ORDINANCE AMENDING ORDINANCE NO. 1525, AS AMENDED, AND KNOWN AS THE "ZONING CODE", BY DEALING WITH A CHANGE OF ZONING ON TRACT 'A' AND TRACT 'B', BLOCK 5, 'MACFARLANE HOMESTEAD' AND ST. ALBANS PARK', CORAL GABLES, FLORIDA; AND REPEALING ALL ORDINANCES INCONSISTENT HEREWITH.

WHEREAS, after Notice of Public Hearing duly published and notification of all property owners of record within three hundred feet (300'), a public hearing was held before the Planning and Zoning Board of The City of Coral Gables, Florida, on May 18, 1970, at which hearing all interested persons were afforded an opportunity to be heard; and:

NOW, THEREFORE, BE IT ORDAINED BY THE COMMISSION OF THE CITY OF CORAL GABLES:

Tract **A'** less the Southeast 11 feet and Tract **B'** less the Southeast 11 feet, Replat of Block 5, of the combined and supplemental map of: "**MacFarlane Homestead Plat**" and "**Si Albans Park**", 251 South Dixie Highway, Coral Gables, Florida.

PASSED AND ADOPTED, **THIS** FOURTEENTH DAY OF JULY, A. D. 1970.

C. L. Dressel, MAYOR

Loretta V. Sheehy, CITY CLERK

07.14.2016 City Commission Submittal

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APPLICABLE ZONING RESOLUTION

WHEREAS, the Property is affected by a number of easements and resolutions, which are obsolete and of no further benefit to the City or the public due to the proposed development, or will be replaced by other easements of record; and

WHEREAS, it is necessary for Gables Station to grant and convey to the City of Coral Gables a non-exclusive easement to provide for construction, maintenance, repair and replacement of sanitary sewer utility facilities to serve the proposed development;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COMMISSION OF THE CITY OF CORAL GABLES:

SECTION 1. That the foregoing “Whereas” clauses are hereby ratified and confirmed as being true and correct and are hereby made a specific part of this Resolution upon the adoption hereof.

SECTION 2. That the Easement and Maintenance Agreement (Oak Avenue Sidewalk), Right of Way Encroachment Agreement, Termination of Easements and Grant of Easement, are hereby authorized in substantially the forms attached hereto as Exhibits “A,” “B,” “C,” and “D.”

SECTION 3. That Gables Station will execute a restrictive covenant in a form acceptable to the City Attorney that ensures the obligations and duties being assumed by Gables Station as part of the documents mentioned above run with the land and apply to successors in interest.

SECTION 4. That the covenant referenced in Section 3, the Easement and Maintenance Agreement (Oak Avenue Sidewalk), the Termination of Easements, and the Grant of Easement will be recorded in the public records of Miami-Dade County by Gables Station, at its sole cost and expense. Gables Station shall provide the City with a certified, recorded copy of these instruments within 90 days of the effective date of this Resolution.

SECTION 5. That the City Commission does hereby authorize the City Manager to execute such documents, with such modifications from the terms as may be approved by the City Manager and City Attorney and are necessary to implement the intent of this Resolution.

SECTION 6. That this Resolution shall become effective immediately upon the date of its passage and adoption herein.

PASSED AND ADOPTED THIS ELEVENTH DAY OF JUNE, A.D., 2013.
(Moved: Quesada / Seconded: Lago)
(Yeas: Keon, Lago, Quesada, Cason)
(Vote: Majority: (4-0) Vote)
(Absent: Kerdyk)
(Agenda Item: H-1)

APPROVED:

JIM CASON
MAYOR

ATTEST:

WALTER L. FOEMAN
CITY CLERK

APPROVED AS TO FORM
AND LEGAL SUFFICIENCY

CRAIG E. LEEN
CITY ATTORNEY

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES,
FL 33134

PROPERTY OWNER

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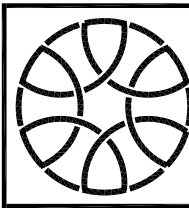
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APPLICABLE ZONING RESOLUTION

CITY OF CORAL GABLES, FLORIDA

RESOLUTION NO. 2013-126

RESOLUTION APPROVING THE CONCEPT FOR A WORK OF PUBLIC ART IN CONJUNCTION WITH THE GABLES STATION PROJECT FRONTING SOUTH DIXIE HIGHWAY (US HIGHWAY NO. 1) BETWEEN OAK AVENUE AND GRAND AVENUE IN FULFILLMENT OF THE ART IN PUBLIC PLACES REQUIREMENT FOR PUBLIC ART IN PRIVATE DEVELOPMENT.

WHEREAS, Gables Station, LLC (the “Developer”) is constructing a new building at on Route 1 between Oak Avenue and Grand Avenue known as Gables Station (the “Project”), and has chosen the option of incorporating art within the development project in fulfillment of the Art in Public Places requirement as set forth in the Zoning Code, Article 3, Division 20; and

WHEREAS, Larry Kirkland (the “Artist”), selected by the Developer, was approved as a qualified artist by the Arts Advisory Panel on October 6, 2011 and the Cultural Development Board on October 18, 2011, in accordance with the City of Coral Gables Art in Public Places: Funding, Goals, and Implementation Guidelines, adopted by the City Commission on September 14, 2010 as Resolution No. 2010-199 (the “Guidelines”); and

WHEREAS, on October 6, 2011, the Artist and the Developer presented a concept proposal of the public art work to the Arts Advisory Panel for review, which recommended acceptance to the Cultural Development Board; and

WHEREAS, on October 18, 2011, the Artist and the Developer presented the concept proposal of the recommended public art work to the Cultural Development Board, which reviewed and accepted the recommendation of the Arts Advisory Panel to recommend the Artist’s concept for a work of public art and determined that it is in keeping with the curatorial goals and programmatic goals as specified in the Guidelines and meets the criteria for evaluating artist concepts as specified in the Guidelines; and

WHEREAS, on December 22, 2011, the Developer presented a concept proposal of the public art work to the Board of Architects for review as to siting, and the Board recommended acceptance; and

WHEREAS, the proposed art piece is estimated to be \$888,970, which exceeds the required 1% of the Project’s estimated budget of \$830,846 as shown on Exhibit A attached hereto. The final amount will be determined at the time the permit is issued, and if the cost of the art piece is less than the 1% requirement, the Developer will pay any balance to the Art in Public Places fund.

NOW, THEREFORE, BE IT RESOLVED BY THE COMMISSION OF THE CITY OF CORAL GABLES, FLORIDA:

SECTION 1. That the foregoing “Whereas” clauses are hereby ratified and confirmed as being true and correct and are hereby made a specific part of this Resolution upon the adoption hereof.

SECTION 2. That the City Commission does hereby authorize the Artist’s concept and proposed preliminary budget for a work of public art in fulfillment of the Art in Public Places requirement, attached hereto as Exhibit A, provided that such work shall meet all other applicable criteria of the Zoning Code, and other applicable codes, statutes, laws, rules, and regulations.

SECTION 3. That this resolution shall become effective immediately upon the date of its passage and adoption herein.

PASSED AND ADOPTED THIS ELEVENTH DAY OF JUNE, A.D., 2013.

(Moved: Keon / Seconded: Lago)
(Yeas: Lago, Keon, Quesada
(Majority: (3-0) Vote)
(Absent: Kerdyk, Cason)
(Agenda Item: H-2)

APPROVED:

Jim Cason
JIM CASON
MAYOR

ATTEST:

Walter J. Foeman
WALTER J. FOEMAN
CITY CLERK

APPROVED AS TO FORM
AND LEGAL SUFFICIENCY

Craig E. Leen
CRAIG E. LEEN
CITY ATTORNEY

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES,
FL 33134

PROPERTY OWNER

NP INTERNATIONAL
2903 Salzedo Street, Coral Gables, FL 33134
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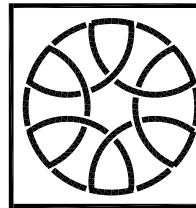
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DATE DESCRIPTION

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PROJECT NAME

GABLES STATION

SCALE

DRAWING

SEC.09

PAGE NUMBER

SECTION 10

DEED

①
Return to
Armon
246 S. Bayshore Dr
#1200 Coconut Grove
This instrument prepared by: #1-33133

Harold L. Lewis, Esq.
Pathman Lewis, LLP
One Biscayne Tower, Suite 2400
2 South Biscayne Boulevard
Miami, Florida 33131
(305) 379-2425

Folio No(s).: 03-4120-027-0020
03-4120-027-0010
03-4120-026-0010

SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED, made this 15th day of December, 2005, between SAMKLE HOLDINGS, INC., a Florida corporation, as "GRANTOR," and GABLES STATION, LLC, a Florida limited liability company, whose mailing address is 2865 S. Bayshore Drive, Suite 1200, Coconut Grove, Florida 33133, as "GRANTEE."

(Whenever used herein, the terms "GRANTOR" and "GRANTEE" include all the parties to this instrument and the heirs, legal representatives and assigns of individuals, and the successors and assigns of corporations.)

WITNESSETH:

That the GRANTOR, for and in consideration of the sum of Ten Dollars (\$10.00) and other good and valuable considerations in hand paid to GRANTOR by said GRANTEE, receipt whereof is hereby acknowledged, by these presents does grant, bargain, sell, alien, remise, release, convey and confirm unto the GRANTEE the following described real property located and situated in the County of Miami-Dade and State of Florida, to wit (the "Property"):

See attached Exhibit "A"

This conveyance is subject to the following:

1. Taxes and assessments for the year 2006 and years subsequent thereto.
2. Conditions, reservations, restrictions, limitations, dedications, easements and other matters of record; provided, however, this shall not serve to reimpose same.
3. Zoning and other governmental restrictions and regulations.

V



CFN 2005R1298975
DR BK 24055 Pgs 4136 - 4138 (3pgs)
RECORDED 12/15/2005 15:34:27
DEED DOC TAX 147,990.00
SURTAX 110,925.00
HARVEY RUVIN, CLERK OF COURT
MIAMI-DADE COUNTY, FLORIDA

TOGETHER with all the tenements, hereditaments and appurtenances thereto belonging or in any way appertaining, including, without warranty of title, the right, title and interest of Grantor, if any, in and to any adjoining or abutting rights of way (including those portions of the former FEC right of way (for its full width) and Ponce de Leon Boulevard (to its centerline) to the North of the Property ("Appurtenant Rights").

TO HAVE AND TO HOLD the same in fee simple forever.

The GRANTOR hereby specially warrants the title to the Property, and will defend the same, against the lawful claims of all persons claiming by, through or under the said GRANTOR but none other; excluding, however, the Appurtenant Rights which are being transferred, assigned and conveyed hereby but without warranty.

IN WITNESS WHEREOF, the GRANTOR has caused this instrument to be executed by its proper officer thereunto duly authorized as of the day and year first above written.

Signed, sealed and delivered
In the presence of:

Sign:
Print Name: Harold L. Lewis
Sign:
Print Name: MICHAEL KLEIN

SAMKLE HOLDINGS, INC.,
a Florida corporation,

By:
Print Name: Raphael Klein
Its: Vice President

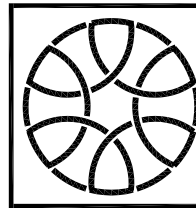
Address: 2101 Brickell Avenue
Suite 2606
Miami, Florida 33129

STATE OF FLORIDA)
COUNTY OF MIAMI-DADE) SS

The foregoing instrument was acknowledged before me on this 15th day of December 2005 by RAPHAEL KLEIN, as Vice President, of SAMKLE HOLDINGS, INC., a Florida corporation, on behalf of the company, who ☒ is personally known to me or ☐ has produced a Florida Driver's License as identification, and who did not take an oath.

Notary Public
My Commission Expires:

r:\samkle holdings\samkle holdings - aron jerry\dca\(-) deed - gables station v.2.doc



OR BK 24055 PG 4138
LAST PAGE

Tracts A and B of the REPLAT OF BLOCK 5 OF THE COMBINED AND SUPPLEMENTAL MAP OF MACFARLANE HOMESTEAD PLAT AND ST. ALBAN'S PARK, according to the Plat recorded in Plat Book 44, Page 22, of the Public Records of Miami-Dade County, Florida, less that portion dedicated for State Road No. 5 (U.S. No. 1).

AND

Beginning at a monument at the most westerly corner of Tract 1, as shown on REVISED PLAT OF PORTIONS OF COMBINED & SUPPLEMENTAL MAP OF MACFARLANE HOMESTEAD PLAT & ST. ALBAN'S PARK (P.B. 5-81) AND AMENDED PLAT OF COCONUT GROVE WAREHOUSE CENTER (P.B. 25-66), according to the Plat thereof, recorded in Plat Book 42, Page 44, of the Public Records of Miami-Dade County, Florida; thence Northeasterly along the Southeasterly right-of-way line of the Florida East Coast Railway, a distance of 158.95 feet to a pipe; thence deflecting to the right 87° 20' 30" and in a Southeasterly direction, a distance of 200.12 feet to a pipe in the Northwesterly line of South Dixie Highway; thence deflecting to the right 92° 39' 15" and in a Southwesterly direction along the Northwesterly line of the aforesaid South Dixie Highway, a distance of 168.28 feet to a pipe at the intersection of the Northwesterly line of South Dixie Highway and the Northeasterly line of Harding Crossing; thence deflecting to the right 90° 1' 9" and in a Northwesterly direction along the Northeasterly line of Harding Crossing, a distance of 199.92 feet to Point of Beginning; less and excepting therefrom the Southeasterly 26 feet thereof, acquired for widening of State Road No. 5, according to Right-of-way map recorded in Plat Book 57, Page 65, of the Public Records of Miami-Dade County, Florida.

AND

That certain street designated as "Harding Crossing" described as that Street bounded on the Northwest by the Southeasterly Right-of-way of the former Florida East Coast Railway, on the Southwest by said Tract A, and on the Southeast by the Northwesterly Right-of-way line of State Road No. 5, and on the Northeast by said Tract 1.

AND

A portion of Tract 1 of REVISED PLAT OF PORTIONS OF COMBINED & SUPPLEMENTAL MAP OF MACFARLANE HOMESTEAD PLAT & ST. ALBAN'S PARK (P.B. 5-81) AND AMENDED PLAT OF COCONUT GROVE WAREHOUSE CENTER (P.B. 25-66), according to the Plat thereof, as recorded in Plat Book 42, at Page 44, of the Public Records of Miami-Dade County, Florida; together with a portion of that street Right-of-way lying Northeasterly of and adjacent to said Tract 1, being more particularly described as follows:

Commence at the Northwesterly line of said Tract 1 extended for a distance of 21.81 feet to the Point of Beginning of the tract of land hereinafter described, said point being situated on the arc of circular curve concave to the Northeast and having for its elements a radius of 350.00 feet and a central angel of 20° 16' 01"; thence run Southeasterly along the arc of said curve for a distance of 123.80 feet to a point of tangency on the Northeasterly line of said Tract 1; said point being 65.04 feet Northwest of (as measured along the Northeasterly line of said Tract 1) the most Easterly corner of said Tract 1; thence run South 50° 47' 44" East along the Northeasterly line of said Tract 1 for a distance of 22.71 feet to a point of curvature of a circular curve to the West and having for its elements a radius of 25.00 feet and a central angle of 95° 03' 47"; thence run Southeasterly along the arc of said curve, for a distance of 41.48 feet to a point of compound curvature with another circular curve concave to the Northwest and having for its elements a radius of 1867.58 feet and a central angle of 1° 31' 41"; thence run Southwesterly along the arc of said curve for a distance of 49.81 feet to a point of tangency; thence run South 45° 47' 44" West for a distance of 100.60 feet to a point; thence run North 46° 50' 28" West for a distance of 174.18 feet to a point on the Northwesterly line of said Tract 1; thence run North 45° 47' 44" East along the Northwesterly line of said Tract 1 for a distance of 187.58 feet to the Point of Beginning. Said tract of land lying and being situated in the City of Coral Gables, Miami-Dade County, Florida.

Exhibit "A"

[Handwritten signature]

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES, FL 33134

PROPERTY OWNER

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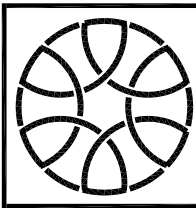
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DATE

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07.14.2016

City Commission Submittal

INFO

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NOTE

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PROJECT NAME

GABLES STATION

SCALE

DRAWING

SEC 10

PAGE NUMBER



GABLES STATION

APPLICANT SUBMITTAL ADDENDUM 07.20.2016
251 S. DIXIE HWY. CORAL GABLES, FL

A DEVELOPMENT BY NP-INTERNATIONAL, USA

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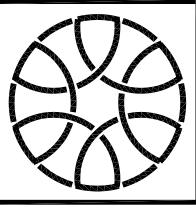
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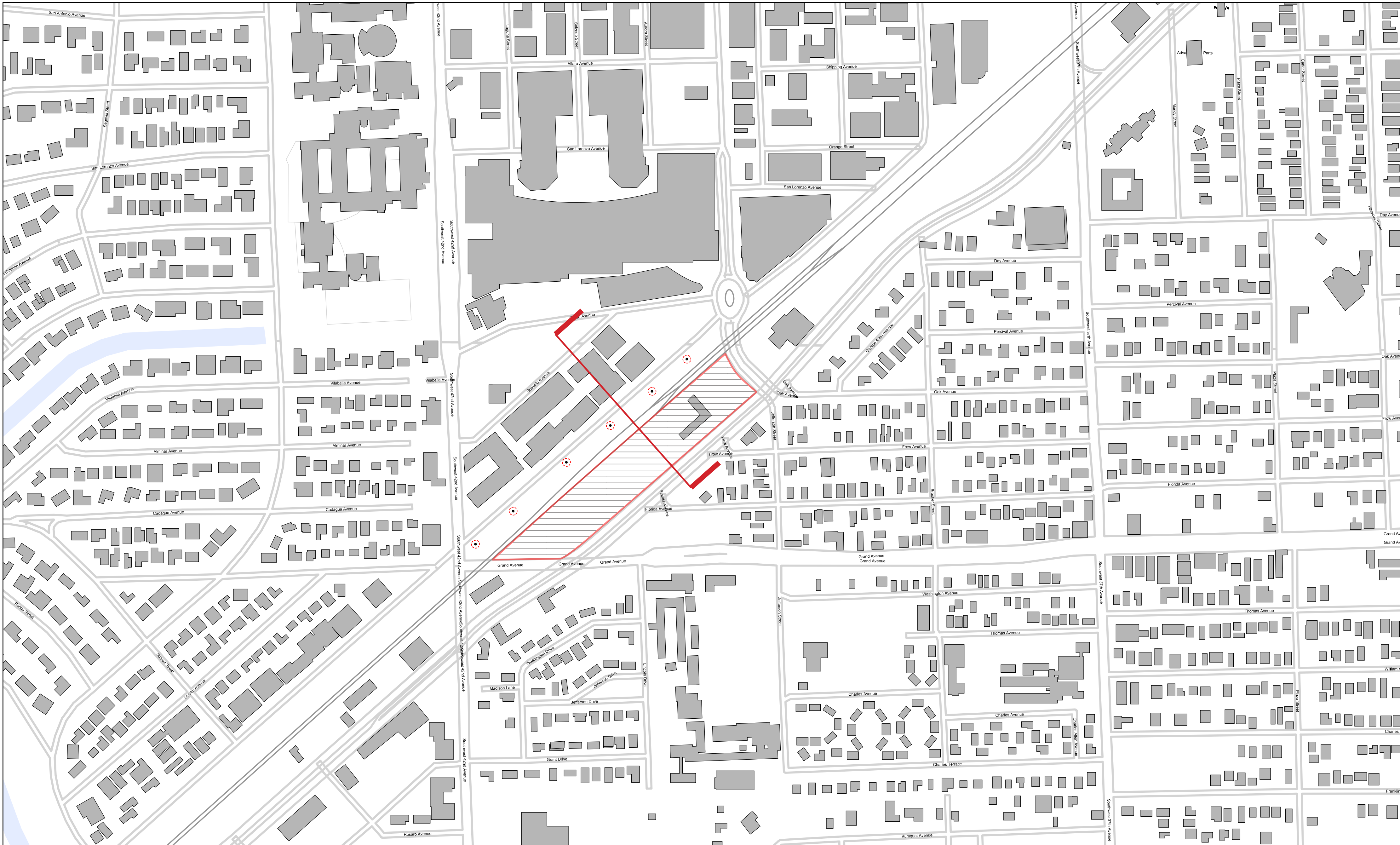
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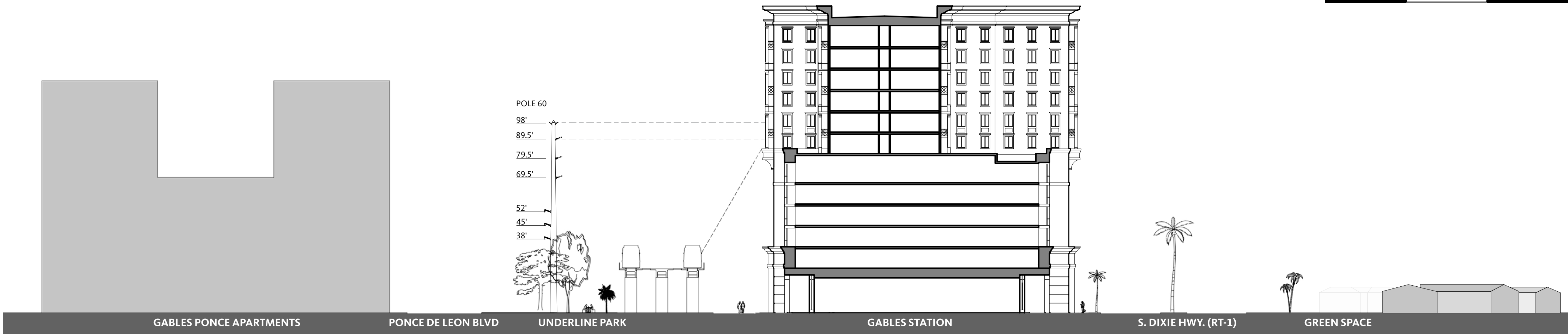
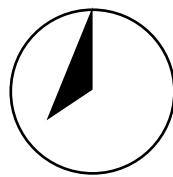
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2

PROPOSED FPL UTILITY POLE LOCATIONS



300 FT 900 FT



50 FT 100 FT

JOB NAME AND LOCATION

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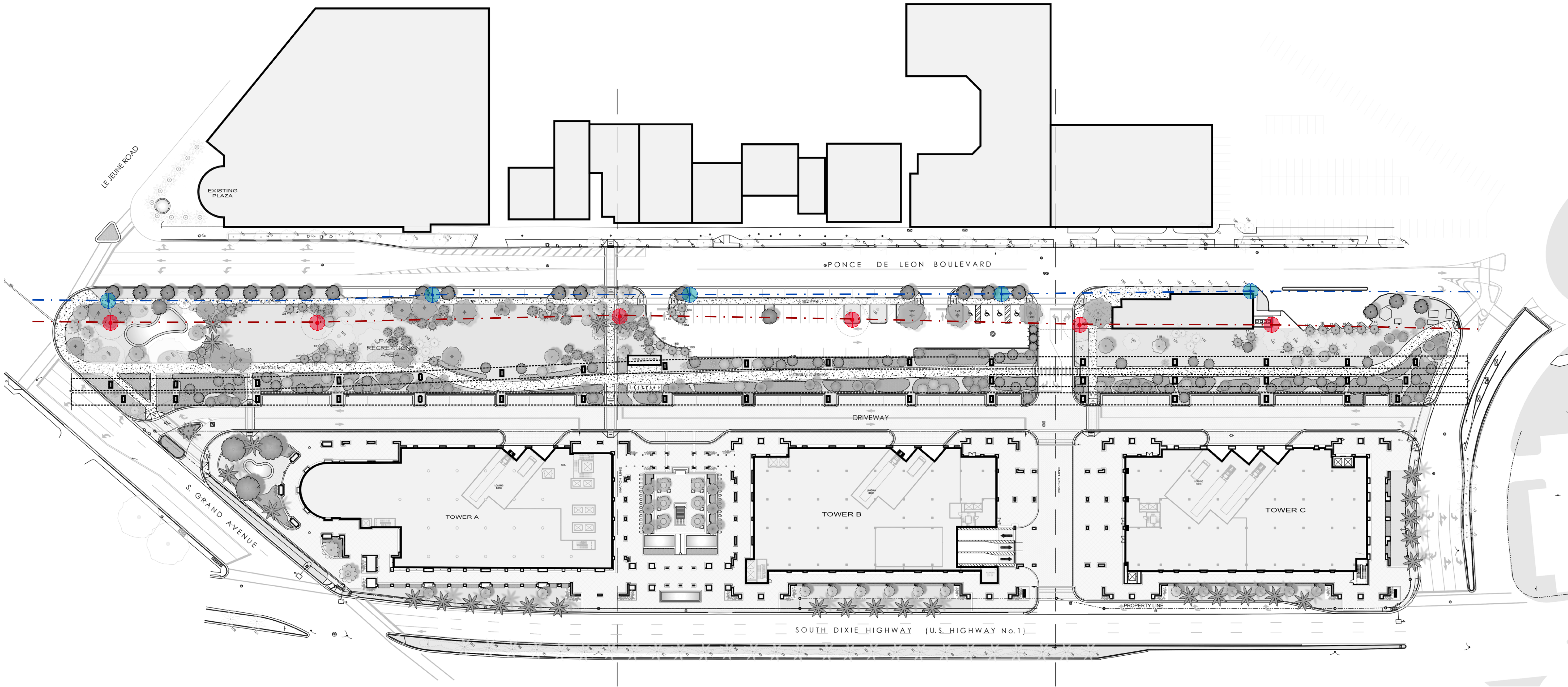
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PROPOSED FPL UTILITY POLE LOCATIONS



KEY

- EXISTING UTILITY POLES
- FUTURE UTILITY POLES
- WIRES

SITE PLAN

SCALE: 1"=50'-0"

JOB NAME AND LOCATION

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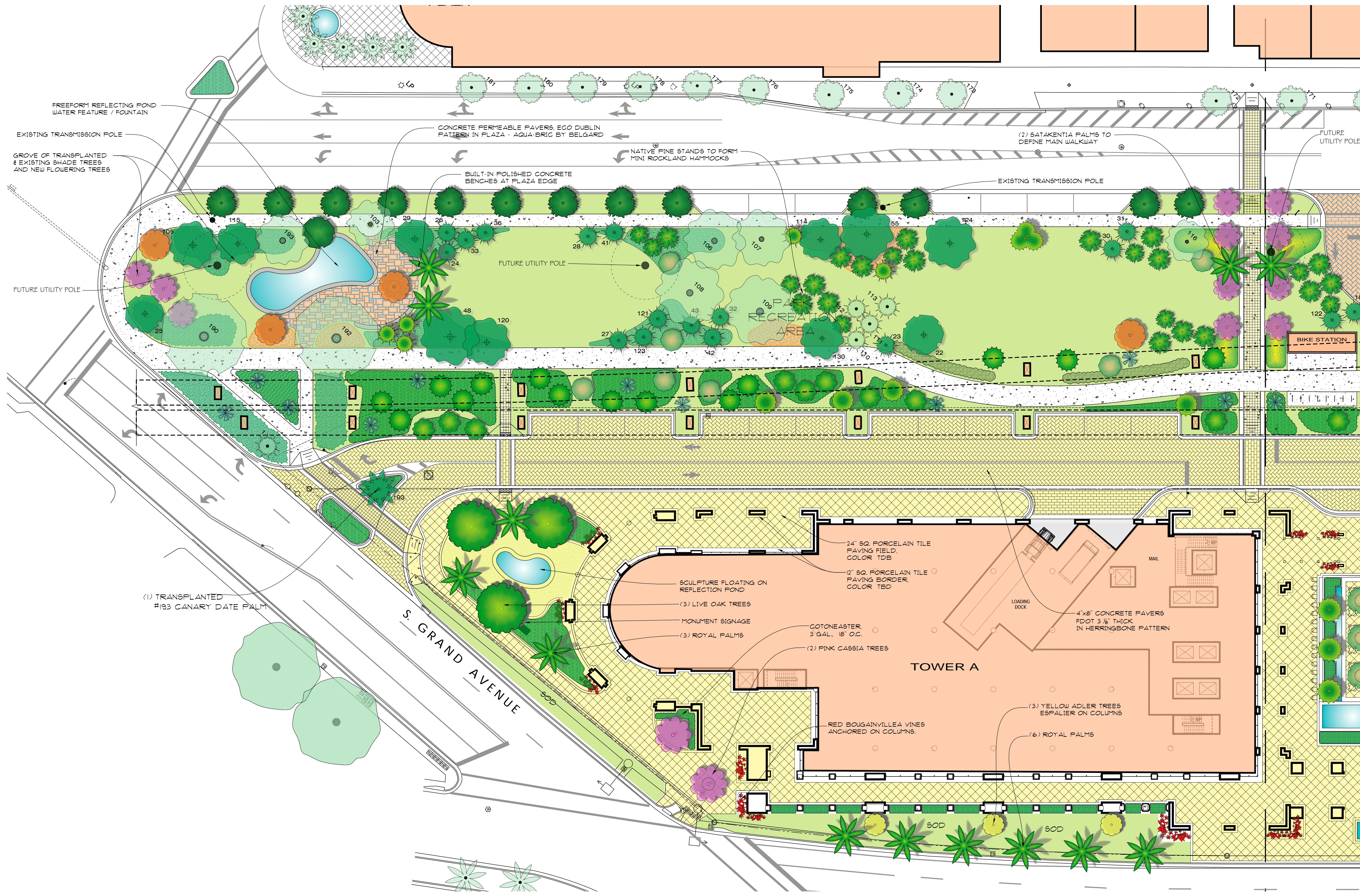
PROJECT NAME GABLES STATION

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PAGE NUMBER

SOUTHERN SITE PLAN - UPDATED TO SHOW PROPOSED FPL UTILITY POLE LOCATIONS



JOB NAME AND LOCATION
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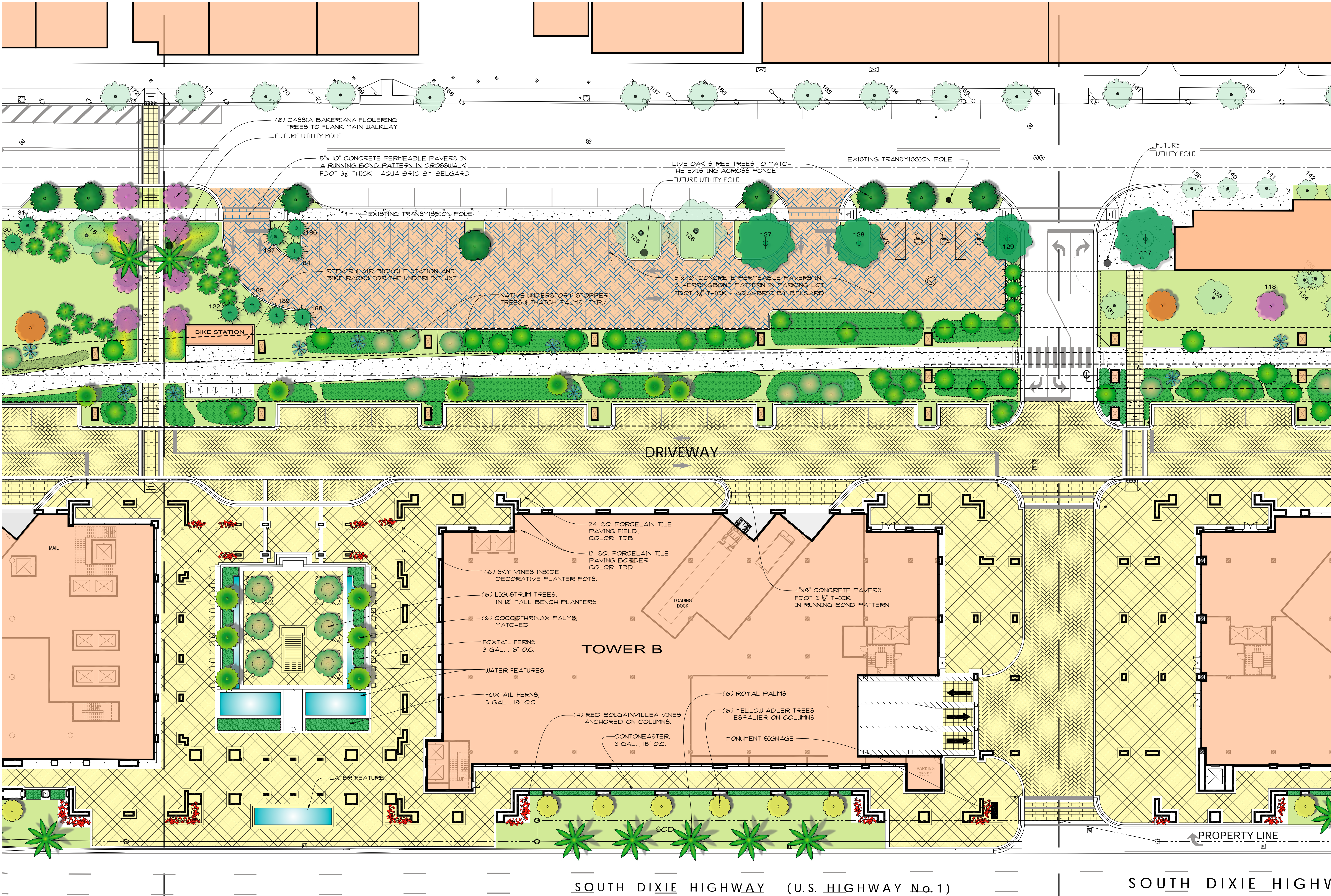
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CENTRAL SITE PLAN - UPDATED TO SHOW PROPOSED FPL UTILITY POLE LOCATIONS

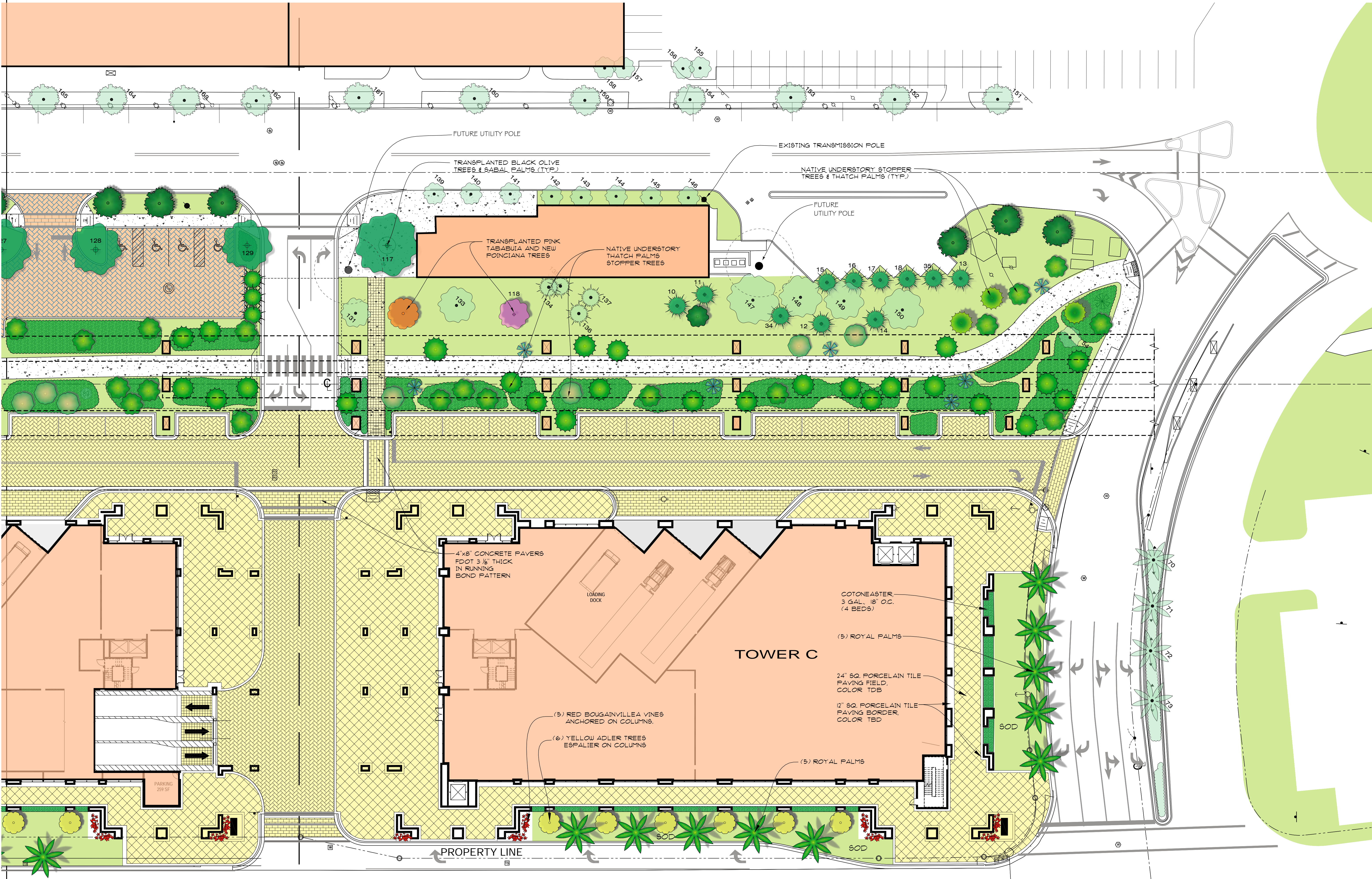


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NORTHERN SITE PLAN - UPDATED TO SHOW PROPOSED FPL UTILITY POLE LOCATIONS



JOB NAME AND LOCATION
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DATE	DESCRIPTION
07.14.2016	City Commission Submittal

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NOTE
 DRAWINGS SCALED FOR 24X36" PRINT

PROJECT NAME
 GABLES STATION

SCALE
 1"=20'-0"

DRAWING
L1.05

PAGE NUMBER

(U.S. HIGHWAY No.1)

SOUTH DIXIE HIGHWAY (U.S. HIGHWAY No.1)

REVISED ART IN PUBLIC PLACES STATEMENT



Our File Number: 000041229.00001
Writer's Direct Dial: (305) 376-6061
Writer's E-Mail Address: MGarcia-Serra@gunster.com

July 19, 2016

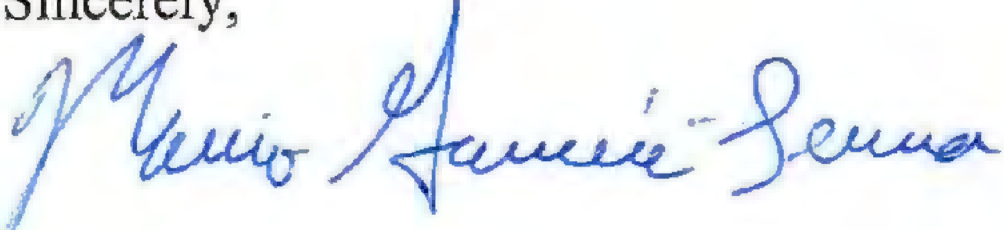
VIA ELECTRONIC MAIL

Mr. Ramon Trias
Planning and Zoning Director
City of Coral Gables
427 Biltmore Way, 2nd Floor
Coral Gables, Florida 33134

Re: “Gables Station” / 251 South Dixie Highway / Revised Art in Public Places Statement

Dear Mr. Trias:

In connection with the City’s Art in Public Places requirement, the Developer intends to incorporate publicly accessible works of art throughout the Project site and the Underline area, some of which will include works of art commemorating and acknowledging the Afro-Bahamian heritage of the McFarlane Homestead and Golden Gate neighborhoods. It is anticipated that these works of public art will be equivalent to the Project’s total monetary obligation under the Art in Public Places Ordinance. Thank you for your attention to this matter.

Sincerely,

Mario Garcia-Serra

MIA_ACTIVE 4421145.1

JOB NAME AND LOCATION

GABLES STATION

251 S. DIXIE HIGHWAY, CORAL GABLES,
FL 33134

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RESPONSES TO CITY OF CORAL GABLES PUBLIC WORKS TRAFFIC ANALYSIS COMMENTS

DAVID PLUMMER & ASSOCIATES

TRANSPORTATION • CIVIL • STRUCTURAL • ENVIRONMENTAL

1750 PONCE DE LEON BOULEVARD, CORAL GABLES, FLORIDA 33134
305 447-0900 • FAX: 305 444-4986 • EMAIL: DPA@DPLUMMER.COM

Responses to City of Coral Gables Public Works Traffic Analysis Comments
(July 15th, 2016)

Re: Second Review of Gables Station Traffic Impact Analysis Dated July 2016

7. Page 25: In Exhibit 11 (Continued), first entry addresses the project driveway connecting to South Ponce de Leon Blvd. near the 4550 block.
- a. As unsignalized, is the driveway intersection analysis based on a two-lane driveway approach (left turn and right turn lanes) and a stop sign only for the driveway?

Response: This driveway was analyzed using a right-turn only lane at the NE approach. This the only stopped controlled approach at this intersection.

Reviewers Response: Page 43 of the PDF file in Appendix A shows a left and right turn lane at the site drive junction with South Ponce de Leon Blvd. Also note that this configuration does not match that shown on Page 307 of the PDF file in Appendix G which shows one lane in each direction with a right turn only to NE bound South Ponce de Leon Blvd. Also note that the site traffic distribution in Figure 10 shows both left and right turn traffic from the site drive to South Ponce de Leon Blvd. The treatment of traffic from the drive to external destinations needs to be reconciled with drive geometry and lanes, turn movements permitted, site traffic distribution, and traffic LOS analyses.

Response: Since comment 7.a is referring to South Ponce de Leon Boulevard, the response refers to the project driveway accessing that roadway, which will be a right-in / right-out driveway.

The project driveway accessing East Ponce de Leon Boulevard will be a full access driveway with a two lane approach (a left-turn lane and a right-turn lane).

The intersection analysis in the report correctly reflect the lane configurations for both driveways. Page 43 of the pdf in Appendix A shows the correct lane configuration for both driveways. Exhibit 10 also shows the correct vehicle distribution at both driveways. Please note that Page 307 of the pdf in Appendix G is only a schematic drawing of the proposed treatment for the M-path crossing at the project driveway.

8. Page 27: Under Section 5.0 – Circulation Plan:

- a. How will the crossing of the greenway trail along the Metrorail corridor be treated where it crosses the driveway?

Response: The proposed crossing is included in Appendix G.

Reviewers Response: This diagram does not match the site diagram as shown on page 43 of PDF file in Appendix A.

Response: The diagrams included in Appendix G is a schematic drawing of the proposed treatment for the M-path crossing at the project driveway. It is not meant to reflect the proposed driveway geometry or lane configurations.

- b. Verify that site roadway geometry will accommodate the proposed truck loading zones without impeding site circulation and for reasonable truck movements to enter and depart from truck bays.

Response: An Autoturn analysis has been completed and included in Appendix G.

Reviewers Response: Refer to Pages 296-307 of PDF file of the revised traffic report dated July 2015. Note that these Autoturn schematics don’t reflect the relocation of the site drive to South Ponce de Leon Blvd. to a point further east in the site:

Please clarify that the site layout will be modified to accommodate truck turning radius apparent conflicts with:

- i. Columns in the truck bay vicinity.
ii. Metrorail columns.
iii. Island adjacent to LeJeune Road at west end of site drive.
iv. Island adjacent to Ponce de Leon Blvd. at east end of site drive.

Response: The revised Autoturn analysis has been completed for the updated site plan (see attached Exhibits). Although the vast majority of trucks accessing the site are expected to be SU-30s and WB-40s, we have used a WB-50 as the design vehicle for a conservative analysis. During final site design, we will work with the architect and structural engineer to improve truck access from Grand Avenue and to relocate/remove columns in the truck bays.

w:\15\15228\response to comments july 2016\response to comments_second review\response to city comments_gables station.docx



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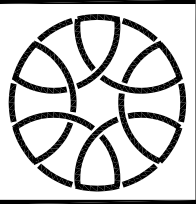
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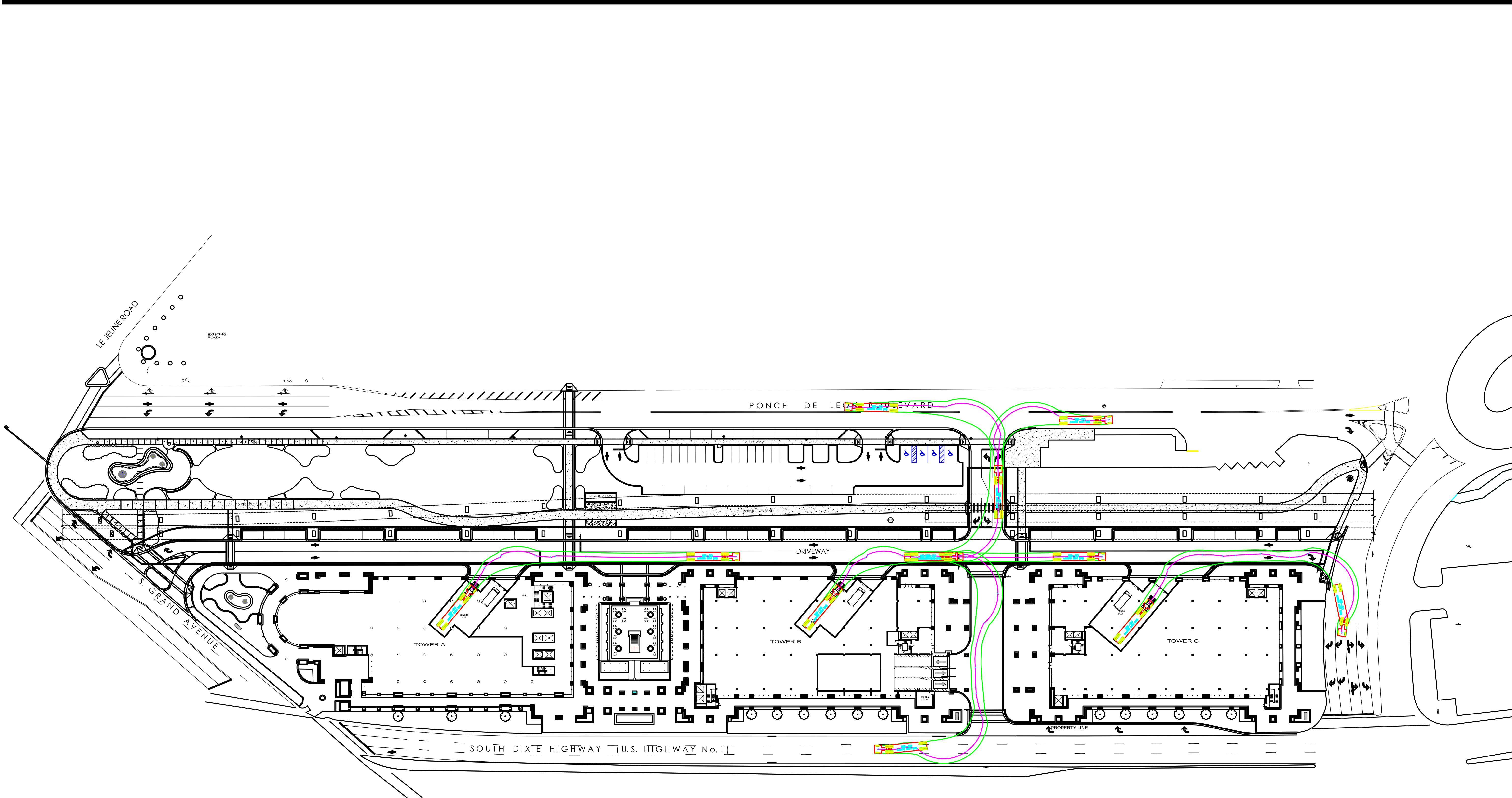
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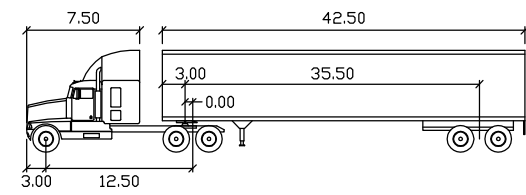
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CERTIFICATE OF AUTHORIZATION EB 2690

Design Vehicle: WB-50

Green Line Denotes Tracking Path
Magenta Line Denotes Center Path



WB-50	feet		
Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Tractor Length	: 8.50	Steering Angle	: 17.7
Tractor Track	: 8.00	Articulating Angle	: 70.0
Trailer Track	: 8.50		

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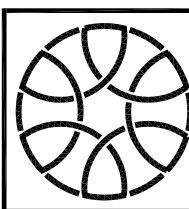
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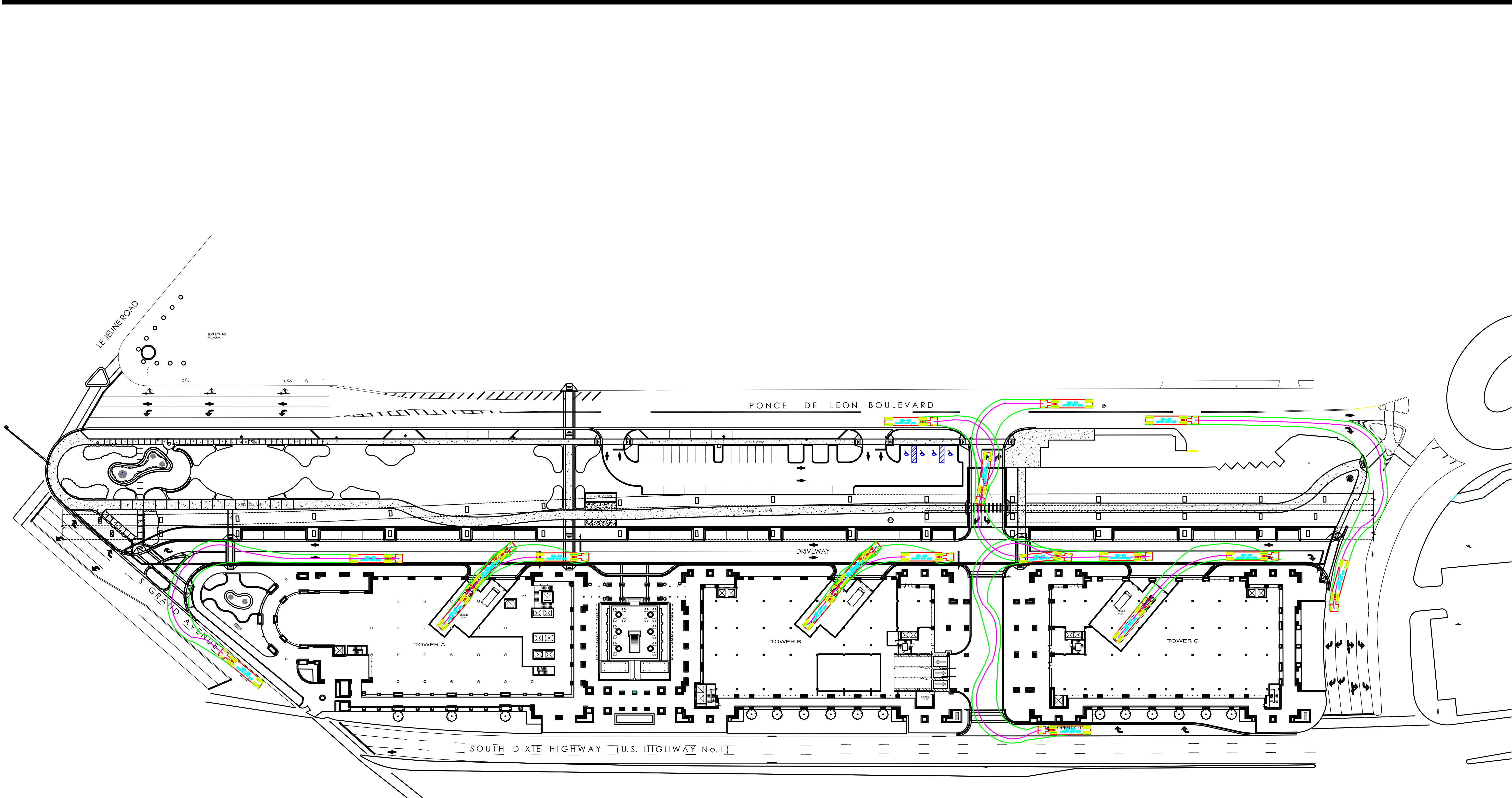
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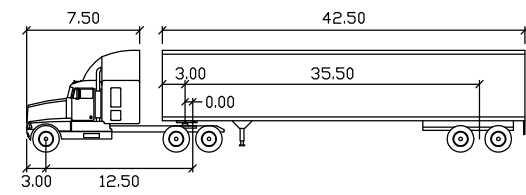
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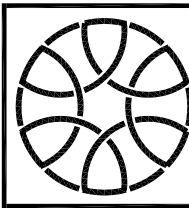
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